

CITY OF COMPTON

COMPTON STATION

SPECIFIC PLAN

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Metro



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COMPTON

building a better downtown

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Table of Contents

Chapter 1: Introduction

- 1.1 Introduction and Purpose
- 1.2 Specific Plan Location
- 1.3 Background and History
- 1.4 Community Outreach Process and Guidance
- 1.5 Goals and Actions
- 1.6 Organization and How to Use this Specific Plan

Chapter 2: Urban Design Framework

- 2.1 Introduction and Purpose
- 2.2 Specific Plan Area Districts and Land Use Growth Projections
- 2.3 Downtown Vision and Urban Design Framework
- 2.4 Civic Center
- 2.5 Commercial Core
- 2.6 Historic Core
- 2.7 The Compton Walk
- 2.8 Willowbrook Avenue
- 2.9 Alameda East

Ch 3: Zoning, Development, and Design Standards

- 3.1 Administration and Procedures
- 3.2 Zoning Districts
- 3.3 Downtown Core (DC) Zone
- 3.4 Downtown Transition (DT) Zone
- 3.5 Residential Urban (RU) Zone
- 3.6 Neighborhood Corridor (NC) Zone
- 3.7 Urban Flexible (UF) Zone
- 3.8 Frontage Standards
- 3.9 Standards for Active Ground Floor Design
- 3.10 Public Art Requirements
- 3.11 Inclusionary Zoning Requirement
- 3.12 Public Benefit Concessions Menu
- 3.13 Signage Standards

Ch 4: Design Guidelines

- 4.1 Introduction and Use of Design Guidelines
- 4.2 Building Siting and Orientation
- 4.3 Building Modulation and Articulation
- 4.4 Architectural Theme, Materials, Finishes and Color
- 4.5 On-Site Circulation
- 4.6 Parking Facility Design
- 4.7 Façade Enhancements and Activities

- 4.8 Residential Open Space and Amenities
- 4.9 Residential Privacy
- 4.10 Green and Active Roofs
- 4.11 Walls and Fences
- 4.12 Exterior Lighting
- 4.13 Outdoor Service, Storage, and Loading Areas
- 4.14 Compton Walk Placemaking
- 4.15 Compton Walk Private Development Interfacing

Ch 5: Mobility and Parking

- 5.1 Mobility Goals and Strategy
- 5.2 Street Network
- 5.3 Compton Boulevard
- 5.4 Willowbrook Avenue
- 5.5 Alameda East
- 5.6 Acacia Avenue
- 5.7 Commercial Core New Streets Network
- 5.8 New Streets
- 5.9 Pedestrian Mall
- 5.10 Mid-Block Passthrough
- 5.11 Shared Alley
- 5.12 Bicycle Network
- 5.13 Vehicular Access and Gateways
- 5.14 Parking Context and Parking District Strategy
- 5.15 Parking and Transportation Demand Management (TDM) Regulations

Ch 6: Infrastructure

- 6.1 Introduction
- 6.2 Water
- 6.3 Sewer
- 6.4 Storm Drain
- 6.5 Electrical
- 6.6 Natural Gas
- 6.7 Telecommunications

Ch 7: Implementation and Financing Strategies

- 7.1 Implementation and Financing Strategies
- 7.2 Downtown Compton Business Improvement District
- 7.3 Public Land Development
- 7.4 Arts and Culture Promotion
- 7.5 Historic Resources
- 7.6 Funding Sources
- 7.7 Capital Project Implementation

Appendix: Compton Walk Streetscape Design Concept

- A.1 Streetscape Design Concept and Theme
- A.2 'The Score Sheet' Base Streetscape Improvements
- A.3 Streetscape Customization: Historic, Art, and Placemaking Features
- A.4 Greening Features

List of Figures

Chapter 1: Introduction

- 1-1. Regional Setting and Transit Connections
- 1-2. Key Specific Plan Chapters by User Group

Chapter 2: Urban Design Framework

- 2-1. Districts
- 2-2. Land Use Growth Projections
- 2-3. Land Use Growth Projections Calculations
- 2-4. Urban Design Framework Axonometric View
- 2-5. Urban Design Framework Plan View
- 2-6. Civic Center Plan and Key Features
- 2-7. Commercial Core Plan and Key Features
- 2-8. Historic Core Plan and Key Features
- 2-9. Streetscape 'Score Sheet' Programming Concept
- 2-10. Overall Compton Walk Routing and 'Places' Concept
- 2-11. Compton Walk Concept
- 2-12. Willowbrook Avenue Plan and Key Features
- 2-13. Alameda East Greenway Plan and Key Features

Chapter 3: Zoning, Development and Design Standards

- 3-1. Designated Approving Authority
- 3-2. Type of Administrative Relief Allowed
- 3-3. Zoning Map
- 3-4. Permitted Uses, Downtown Core (DC) Zone
- 3-5. Development Standards, Downtown Core (DC) Zone
- 3-6. Projections into Setback Areas, Downtown Core (DC) Zone
- 3-7. Permitted Uses, Downtown Transition (DT) Zone
- 3-8. Development Standards, Downtown Transition (DT) Zone
- 3-9. Projections into Setback Areas, Downtown Transition (DT) Zone
- 3-10. Permitted Uses, Residential Urban (RU) Zone
- 3-11. Development Standards, Residential Urban (RU) Zone
- 3-12. Projections into Setback Areas, Residential Urban (RU) Zone
- 3-13. Permitted Uses, Neighborhood Corridor (NC) Zone
- 3-14. Development Standards, Neighborhood Corridor (NC) Zone
- 3-15. Projections into Setback Areas, Neighborhood Corridor (NC) Zone
- 3-16. Permitted Uses, Urban Flexible (UF) Zone
- 3-17. Development Standards, Urban Flexible (UF) Zone
- 3-18. Projections into Setback Areas, Urban Flexible (UF) Zone

- 3-19. Arcade Frontage Standards
- 3-20. Gallery Frontage Standards
- 3-21. Terrace Frontage Standards
- 3-22. Shopfront Frontage Standards
- 3-23. Parking Frontage Standards
- 3-24. Lobby Frontage Standards
- 3-25. Stoop/Porch Frontage Standards
- 3-26. Front Yard Frontage Standards
- 3-27. Sign Matrix
- 3-28. Permitted Signs

Chapter 4: Design Guidelines

- 4-1. Outdoor dining standards for areas other than the Compton Walk
- 4-2. Streetscape 'Score Sheet' Programming Concept

Chapter 5: Mobility and Parking

- 5-1. Downtown Street Network and Modified Corridors
- 5-2. Compton Boulevard Typical Cross-Section, Acacia Avenue to Santa Fe Avenue
- 5-3. Parallel Corridors to Compton Boulevard
- 5-4. Compton Boulevard Typical Segment and Key Features
- 5-5. Compton Walk Streetscape Zones
- 5-6. Typical 'Score Sheet' Streetscape
- 5-7. Compton Walk Landscape Infrastructure Systems
- 5-8. Common Safety Issues at Willowbrook Avenue Intersections
- 5-9. Willowbrook Avenue Design Alternatives
- 5-10. Willowbrook Avenue Preferred Alternative (B-Reverse Couplet) Plan and Key Features
- 5-11. North Willowbrook Avenue East (Palmer to Compton) Typical Cross-Section
- 5-12. North Willowbrook Avenue East (Rosecrans and Palmer) Typical Cross-Section
- 5-13. South Willowbrook Avenue East (Compton to Myrrh) Typical Cross-Section
- 5-14. Willowbrook Avenue West Typical Cross-Section
- 5-15. Willowbrook Avenue Couplet Extents and End Transitions
- 5-16. Alameda Street East – Existing Cross-Section
- 5-17. Alameda East Greenway Typical Cross-Section (Preferred Alternative)
- 5-18. Alameda East Greenway Typical Cross-Section (Reduced Greenway Alternative)
- 5-19. Alameda East Greenway Plan and Key Features
- 5-20. Acacia Avenue Typical Cross-Section, Compton Boulevard to Myrrh Avenue
- 5-21. Illustration of Commercial Core Street and Open Space Network Standards
- 5-22. Commercial Core New Street without Center Turn Lane Typical Cross-Section
- 5-23. Commercial Core New Street with Center Turn Lane Typical Cross-Section
- 5-24. Pedestrian Mall Typical Cross-Section
- 5-25. Mid-Block Passthrough Typical Cross-Section
- 5-26. Shared Alley Typical Cross-Section
- 5-27. Bicycle Network
- 5-28. Downtown Access and Gateways
- 5-29. Downtown Parking Locations
- 5-30. Minimum Bicycle Parking Requirements

Chapter 6: Infrastructure

- 6-1. Development Capacity Map.
- 6-2. Development Capacity by Infrastructure Analysis Area
- 6-3. Water System.
- 6-4. Sewer System.
- 6-5. Storm Drain System.
- 6-6. Gas System.

Chapter 7: Implementation and Financing Strategies

- 7-1. Funding Sources
- 7-2. Capital Projects
- 7-3. Capital Improvement Projects with Funding Sources

Appendix: Compton Walk Streetscape Design Concept

- A-1. Overall Compton Walk Diagram and 'Score Sheet' Programming Concept
- A-2. Typical 'Score Sheet' Streetscape
- A-3. Compton Walk Landscape Infrastructure Systems.

Chapter 1: Introduction

1.1 Introduction and Purpose

The Compton Station Specific Plan lays out a vision, regulations, and implementation plan to build a better Downtown Compton, making it into a world-class destination that all our residents can enjoy.

What is a Specific Plan?

A Specific Plan is a regulatory tool that local governments use to implement their General Plan and to guide development in a localized area. While the General Plan is the overall guide for growth and development in a community, the goal of a Specific Plan is to focus on the unique characteristics of a special area by customizing the planning process and land use regulations to that area. The authority for preparation and adoption of specific plans is set forth in the California Government Code, Sections 65450 through 65457. The California Government Code authorizes jurisdictions to adopt specific plans by ordinance as regulatory documents. The law allows adoption of Specific Plans as may be required for the implementation of the General Plan.

1.2 Specific Plan Location

Compton is a city of approximately 100,000 residents located 10 miles south of Downtown Los Angeles, between the I-110, I-105, I-710, and CA-91 freeways. Compton is bordered by the City of Carson to the south, unincorporated Willowbrook to the west and north, the City of Lynwood to the Northeast, unincorporated East Compton to the east, and the City of Long Beach to the southeast.

The Metro A Line (Blue) provides all-day, frequent rapid transit access from two stations in the City, Compton and Artesia Stations, to the Los Angeles metropolitan area including Downtown Los Angeles, and Downtown Long Beach. It recently underwent a \$1 billion refurbishment to improve speed, reliability, and customer experience.

The Compton Station Specific Plan Area encompasses 575.5 acres, encompassing roughly one half-mile buffer from Compton Station. The Plan Area is bordered by Matthisen Avenue to the west, Cressey Street, Douglas Street, and Peck Street to the north, Santa Fe Avenue to the east, and Myrrh Street, Indigo Street, Laurel Street, and Compton Boulevard to the south. The major thoroughfares within the Plan Area are Rosecrans Avenue, Compton Boulevard, Alameda Street, and Willowbrook Avenue, which runs through the middle of the Plan Area. Freeway access to the Plan Area is provided to I-110 by Compton Boulevard, to CA-91 by Alameda Street and Santa Fe Avenue, and to I-710 by Rosecrans Avenue.

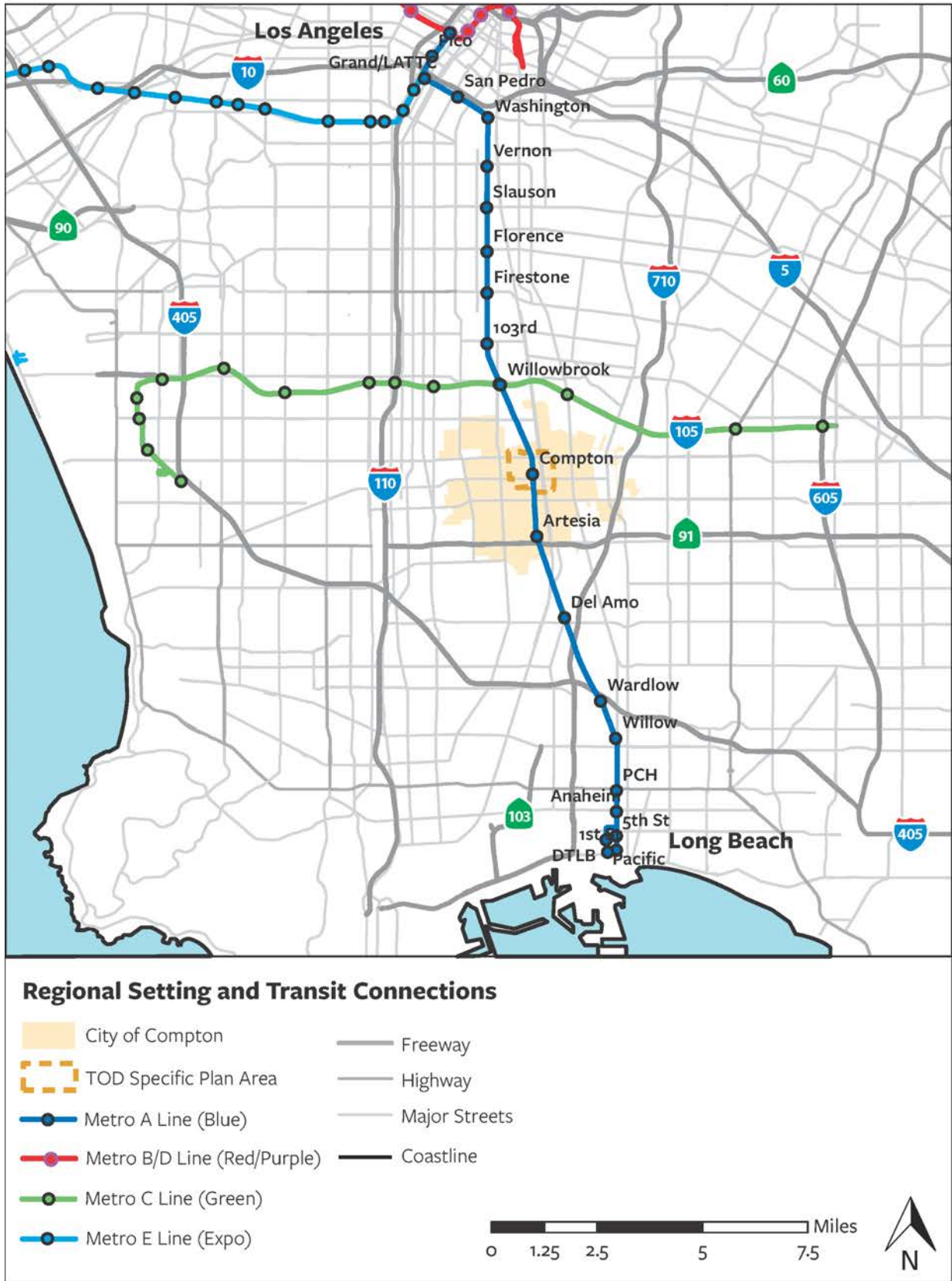


Figure 1-1. Regional Setting and Transit Connections.

1.3 Background and History

Compton is one of the oldest cities in Southern California, the eighth to incorporate. Being located within a day's walk of Los Angeles and Wilmington, and with connections to the Pacific Electric Long Beach Line along with Southern Pacific and Union Pacific freight rail lines in the early 20th century led to Compton being nicknamed the "Hub City". With a boom shortly after World War II, and a decline in the 70's and 80's, Compton has seen its share of ups and downs.



Compton Boulevard as seen in 1962 looking west from Alameda Street (left). Source: Compton (Images of America) by Robert Lee Johnson. The Renaissance Monument on Compton Boulevard (right) was dedicated to Dr. Martin Luther King, Jr. and Cesar Chavez in 2001 as a symbol of Black and Latino unity.

Despite its turbulent history, the City of Compton is a place that continues to inspire devotion and pride. The City has been an important venue for black political leadership with the election of Douglas Dollarhide as the first black mayor in California, Doris A. Davis as the first female black mayor in the United States, and Aja Brown becoming the youngest elected mayor in the state of California. The City is known globally as the center of West Coast Hip-Hop, and its high school basketball games are legendary. Famous people from Compton include:

- Duke Snider, MLB Hall of Famer (Compton HS)
- William Hanna, cartoonist/ animator, co-founder of Hanna-Barbera (Compton HS)
- Kendrick Lamar, rapper (Centennial HS)
- N.W.A., rap group: Eazy E, MC Ren, Ice Cube, Dr. Dre, Arabian Prince, DJ Yella
- Coolio, rapper
- Leslie Sykes, KABC-TV midday news anchor
- Larry Allen, Hall of Fame NFL player (Centennial HS)
- Tyson Chandler, NBA player (Dominguez HS)
- DeMar DeRozan, NBA player (Compton HS)
- Tayshaun Prince, former NBA player (Dominguez HS)
- Venus and Serena Williams, tennis players
- Ava DuVernay, film director/writer
- Richard Sherman, NFL player (Dominguez HS)
- Vince Staples, rapper

Recent years have opened new opportunities for Compton. The development of the Metro A Line (formerly Blue Line) in 1990 uses most of the former Pacific Electric Long Beach Line right-of-way, connecting Compton to Long Beach, Los Angeles, and beyond. The Compton Gateway Center, opened in 2007, has added more retail options to the city, building upon the existing Compton Renaissance Plaza. The completion of the Martin Luther King Jr. Transit Center in 2011 has increased the safety and connectivity of public transit, prompting new housing developments to be built in the station area and around the City. The City also has a brand new community center, the first Major League Baseball Urban Youth Academy, and the forthcoming Dr. Dre Performing Arts Center at Compton High School. The future has never looked brighter for The Hub City.

1.4 Community Outreach Process and Guidance

A key component of developing this Specific Plan has been engagement with and solicitation of input from the public. This consisted of workshops, stakeholder meetings, surveys (in-person and online), and information shared online via a website (www.nextstopcompton.org) and social media (@nextstopcompton on Instagram/Facebook).

Prior to the pandemic, some in-person outreach was conducted. Great comments from the City's youth and other music fans were collected at the Compton Open Mic Night on September 26, 2019, at Dollarhide Community Center. Another focus group meeting was held at Compton YouthBuild on September 30 of the same year. Surveys were collected during the 2019 Christmas parade. Finally, interviews were conducted with several community leaders, representing resident groups, the local creative community, business owners and property owners.



Pre-pandemic outreach activities at Compton Youth Build (left) and the Christmas parade (right).

The early outreach process confirmed a vision that Compton deserves to have a great downtown, one which reflects the values of the community and the contributions the city and those hailing from there have made to the larger world. Those values which should be reflected were:

- **Brother’s Keeper:** Compton is a family-oriented community where people look out for each other.
- **Compton Proud:** Compton is proud of its unique and historic community.
- **Hub of Culture:** Compton creates culture that spreads worldwide, yet contributes to the city.
- **Building the Future:** Compton invests in educational, artistic, recreational, and career opportunities for its youth.

The public also ranked its priorities for the downtown, which were in order of most to least important:

1. Maintenance and Security
2. Housing
3. Retail Amenities and Entertainment
4. Community Gardens and Urban Greening
5. Pedestrian and Bicycle Mobility
6. Arts and Culture
7. Quality Job Creation

Although arts and culture and job creation ranked lower than other topics, stakeholder interviews revealed that community leaders believed that the cultivation and celebration of culture could be a key ingredient to achieving many of the other objectives of the plan.

In April 2020, during the COVID-19 pandemic, the City and consultant team organized a virtual workshop consisting of an explanatory video, with questions posed by community stakeholders, and an accompanying survey. The purpose of this workshop was to evaluate different features of three alternatives (the Cultural Alternative, the Greening Alternative, and the Commercial Alternative) for the downtown area. Meetings were also held with the Stakeholders Advisory Committee, Planning Commission and City Council in 2020 to receive feedback on these three alternatives. The ultimate Downtown Urban Design Framework (Section 2.3) is based on this feedback. An additional Stakeholders Advisory Committee meeting was also held in summer 2020 to review the zoning, mobility strategy, and equity-focused initiatives.

The draft Specific Plan was reviewed by the Stakeholders Advisory Committee in April 2022; the Plan was then made available to the public for review with the Program Environmental Impact Report. A final virtual public workshop was held to gather the community’s feedback, support, and concerns prior to hearings with the Planning Commission and City Council.

Throughout the process, the Next Stop Compton social media accounts allowed the community the opportunity to follow the progress of the plan in real time, express their excitement, hopes, desires and fears for the development of Downtown Compton.

1.5 Goals and Policies

Vision: In order to **build a better downtown, transforming it into a world-class destination that all our community can enjoy**, the Specific Plan sets out a suite of actions organized under six goals, which derive closely from the values and priorities expressed by the community.

Goal #1: An economically active and vibrant station area which attracts investment and provides quality amenities to the community.

Policy 1.1: Establish zoning and development standards which encourage new higher density housing and mixed-use development in commercial portions of the station area (see Chapter 3).

Policy 1.2: Allow development of appropriate uses, including multi-family housing, by-right (see Chapter 3).

Policy 1.3: Encourage the development of sit-down restaurants and other retail amenities strongly desired by the public. Provide favorable development standards for these uses, such as allowing outdoor dining and reducing parking requirements (see Chapter 3 and Section 4.7).

Goal #2: A safe and well-maintained Downtown where people enjoy public space.

Policy 2.1: Establish a business improvement district to maintain, clean, promote and provide security for the Downtown (see Section 7.2).

Policy 2.2: Create a range of comfortable and attractive publicly-accessible open spaces in large developments in the Downtown (see Chapters 2 and 7, Sections 3.3, 4.14, 4.15).

Policy 2.3: Ensure a mix of uses and orient structures to encourage widespread daytime and nighttime use of public spaces (see Chapter 4, Sections 3.3, 3.4, 3.8, 3.9).

Policy 2.4: Encourage new buildings to be multi-story to increase “eyes on the street” and to provide opportunities for increased residential density (see Section 3.3).

Policy 2.5: Enforce design standards which require appropriate levels of lighting, and avoid blank walls and other underutilized spaces (see Sections 3.8, 4.12).

Policy 2.6: Design streets that are intended for pedestrian activity and low vehicle speeds (see Chapter 5).

Policy 2.7: Address the safety of streets and intersections, including the intersections located along the Metro A Line and Alameda rail corridors (see Chapter 5).

Policy 2.8: Avoid dead spaces (hidden areas) that cannot be seen from the street, storefronts, or residences (see Section 3.8).

Goal #3: A built environment and programming which reflect the history, creativity, and excellence of Compton.

Policy 3.1: Construct, maintain and consistently renew the Compton Walk, the primary pedestrian network which connects all major open spaces and is experienced as a journey through cultural

Compton, with places and elements for history and the arts (see Sections 2.7, 4.14, 4.15, 5.3, 7.4).

Policy 3.2: Program cultural events, such as farmers markets, fairs, and concerts, in publicly-accessible open spaces in Downtown (see Section 7.2).

Policy 3.3: Work with partners to establish a cultural center in the station area (see Sections 2.4, 7.3, 7.4).

Policy 3.4: Engage the broader community to develop a museum or other destination venue that showcases field(s) in which the Compton community has excelled (see Sections 2.4, 7.3, 7.4).

Policy 3.5: Direct a portion of project development costs to public art or artistic facilities (see Section 3.10).

Policy 3.6: Create an Arts Commission for the City (see Section 7.4).

Goal #4: A station area which promotes good pedestrian routes and provides calm and attractive networks for pedestrians and bicycles.

Policy 4.1: Improve first/last mile access to the Metro A Line (Blue) Compton Station with additional infrastructure and programs that support walking and biking (see Chapter 5).

Policy 4.2: Create a hierarchy of streets supportive of different modes of transportation, and develop infrastructure for those modes of transportation along the appropriate streets (see Section 5.2).

Policy 4.3: Reconfigure Willowbrook Avenue to reduce safety concerns, improve wayfinding and add low-stress bicycle facilities (see Sections 2.8, 5.4).

Policy 4.4: Develop a greenway on Alameda Street East for recreational/exercise and transportation uses (see Sections 2.9, 5.5).

Policy 4.5: Reduce minimum parking requirements to encourage station area residents to travel less by car (see Chapter 3, Section 5.15).

Policy 4.6: Use existing and new parking structures to create a shared parking district where residents, employees and visitors can park once and circulate around Downtown on foot (see Section 5.14).

Goal #5: Adequate and affordable housing that enables the residents of Compton to remain in the community for the long-term.

Policy 5.1: Make publicly-owned land available for affordable housing development, and pursue development which maximizes the amount of affordable housing produced on those sites (see Section 7.3).

Policy 5.2: Provide by-right development incentives for the development of affordable housing and other public benefit projects (see Section 3.12).

Policy 5.3: Create zoning and development standards which encourage the private production of quality market-rate housing which retains people who grew up in Compton and nearby communities and want to stay (see Chapter 3).

Goal #6: Increased opportunities for Compton residents to create and grow local businesses.

Policy 6.1: Recognize and strengthen Compton as a place where music, art, and entertainment is produced and exhibited to the community and wider world, and support the economic development of local residents engaged in this industry (see Sections 2.2, 2.6, 2.7, 3.10, 4.15, 7.4).

Policy 6.2: Develop an innovation hub which provides amenities and tools for Compton’s digital media producers (see Section 2.6).

Policy 6.3: Explore the opportunity to create a food incubator or other workforce or small business development facility at the corporate yard or other City facility inside the station area (see Section 2.9).

Policy 6.4: Encourage new commercial developments to set aside space for local small businesses and non-profit organizations (see Section 3.12).

Goal #7: Urban design which reinforces the vision and goals of the Specific Plan.

Policy 7.1: Build the Compton Walk as a network of sidewalks and public spaces which provide a high degree of comfort and activation for the pedestrian (see Sections 2.7, 4.14, 4.15, 5.3, 7.4).

Policy 7.2: Require large private developments along the Compton Walk to build publicly-accessible, active, and culturally expressive plazas (see Chapter 3, Section 4.15).

Policy 7.3: Widen sidewalks and buffer sidewalks from vehicular lanes with increased street tree canopy, on-street parking, and bicycle facilities where feasible (see Chapter 5).

Policy 7.4: Require ground-floor building frontages which engage and are transparent to the street and encourage the removal of any blank walls in the station area (see Section 3.8).

Policy 7.5: Encourage outdoor seating along sidewalks and in open spaces (see Section 4.7).

Policy 7.6: Implement design standards and guidelines to ensure that high-density development is livable and attractive (see Chapters 3, 4).

Policy 7.7: Encourage the creation of green and solar roofs (see Chapter 3, Section 4.10).

Policy 7.8: Provide appropriate accent lighting on buildings and landscaping to increase nighttime visual interest and activity (see Section 4.12).

1.6 Specific Plan Organization

The Specific Plan is organized in seven chapters. Beyond this **Chapter 1 (Introduction)**, the document is organized in the following manner:

Chapter 2 (Urban Design Framework) presents the master plan/vision for the Compton Station Specific Plan area, with a particular focus on the area designated as “Downtown Compton,” along Compton Boulevard between Acacia Avenue and Santa Fe Avenue.

Chapter 3 (Zoning, Development and Design Standards) presents regulatory standards for real estate development in the Specific Plan area. The standards regulate both the physical form and use of property.

Chapter 4 (Design Guidelines) presents guidelines to be used in the design review of projects in the Specific Plan area, including arts and cultural projects.

Chapter 5 (Mobility, Parking and Street Standards) contains both conceptual design and regulatory standards for mobility networks in the Specific Plan area, including modifications to current street right-of-ways and the creation of new streets and passageways.

Chapter 6 (Infrastructure) contains an analysis of the infrastructural needs of the area to accommodate the Specific Plan.

Chapter 7 (Implementation) highlights major activities that the City and community can undertake to realize the plan vision and identifies procedures and funding sources.

All of the Specific Plan chapters are relevant and applicable to projects and actions within the Specific Plan area. However, for expediency, Figure 1-2 below highlights the most relevant sections for different users of the Plan with the abbreviation “MR.”

Key Specific Plan Chapters by User Group					
Chapter	Plan Adopting Authority or Community Member	Property Owner/Developer		Project Review Authority	City Operating Dept (non-Planning)
		Downtown/Major Site	Other		
1 (Introduction)	MR			MR	
2 (Urban Design Framework)	MR	MR		MR	MR
3 (Zoning, Development and Design Standards)		MR	MR	MR	
4 (Design Guidelines)		MR	MR	MR	
5 (Mobility, Parking and Street Standards)		MR		MR	MR
6 (Infrastructure)				MR	MR
7 (Implementation)	MR				MR

Chapter 2: Urban Design Framework

2.1 Introduction and Purpose

This chapter describes the Urban Design Framework for the Compton Station area. The Urban Design Framework translates the community vision and Specific Plan Goals and Actions (Section 1.5) into a physical master plan for the development of the Compton Station area to be executed through public and private real estate development projects, and public infrastructure projects. It represents a vision for Compton Station area in 2040, the planning horizon year for this project.

The contents of this chapter are to be interpreted as strong guidelines for all implementing projects. Some flexibility may be warranted as circumstances and conditions change; however, implementation of the key concepts of this Urban Design Framework is critical to ensuring a cohesive, functional, and vibrant Compton Station area.

2.2 Specific Plan Area Planning Districts and Land Use Growth Projections

Figure 2-1 defines the planning districts of the Compton Station Specific Plan area. These districts express the overall structure of the Specific Plan area and represent areas with a common character.

The Civic Center, Commercial Core, and Historic Core districts together make up the historic and future Downtown Compton. The Urban Design Framework for Downtown Compton and its component districts is described in detail in the sections that follow.

Growth projections were created for the purposes of environmental, infrastructure, and transportation analysis. These were determined by consulting the project's market study, recent development trends taking place in the Specific Plan area, and the desire to achieve a critical mass of development that could implement the Urban Design Framework and bring vibrancy and safety to the area. In total (entitled + future entitled projects), growth during the 2022-2040 period is projected to be:

- 2,468 residential units (1,676 market-rate and 792 affordable)
- 137,000 square feet of office/institutional space
- 150 hotel rooms

Total retail space is projected to decline by 25,000 square feet. The projected decline reflects an intended recycling of some large-format, big box stores with mixed-use development and inline stores and restaurants which occupy a much smaller footprint. Despite the square footage decline, the quality, variety, and quantity of retail sales is anticipated to increase markedly.

These growth projections include the development entitled by the 501/601 E. Compton Boulevard Specific Plan, which was adopted in June 2021. As described in Section 2.6, this project is comprised of 290 residential units (232 market-rate and 58 affordable), 14,000 square feet of retail space, and a 40,000 square foot innovation hub office space.

Future entitlement growth projections, analyzed by the environmental analysis accompanying this Specific Plan, are expected to be:

- 2,178 residential units (1,444 market-rate and 734 affordable)
- 97,000 square feet of office/institutional space
- 150 hotel rooms
- -39,000 square feet of retail space

Figure 2-2 and Figure 2-3 describe how this growth is anticipated to be distributed across the Specific Plan area. While it is anticipated that growth during the planning period will be concentrated in the Downtown districts, it is impossible to predict the exact location and extent of growth that will result from the Specific Plan because the majority of land and growth is planned for land which is under private control.

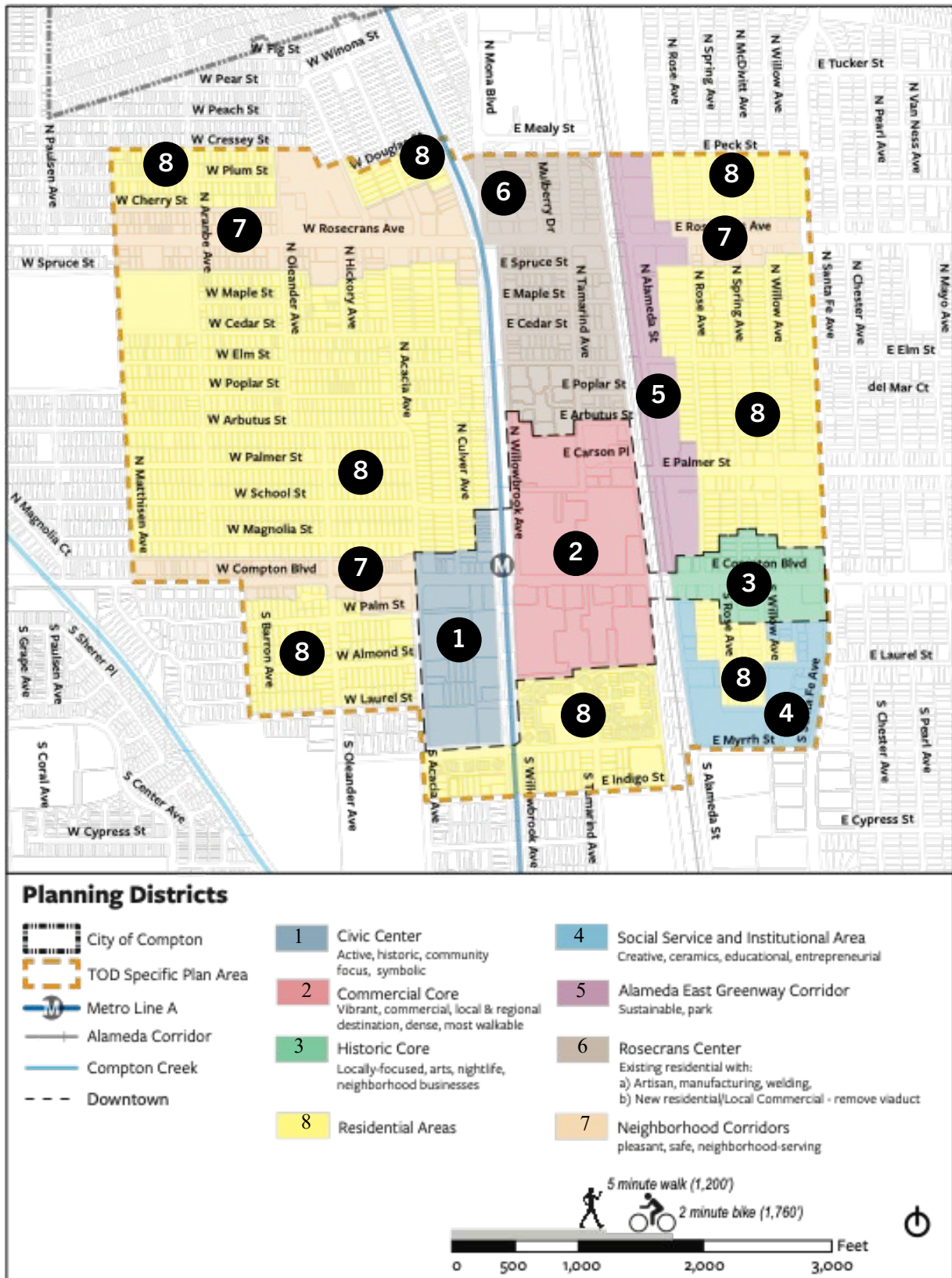


Figure 2-1. Planning Districts.

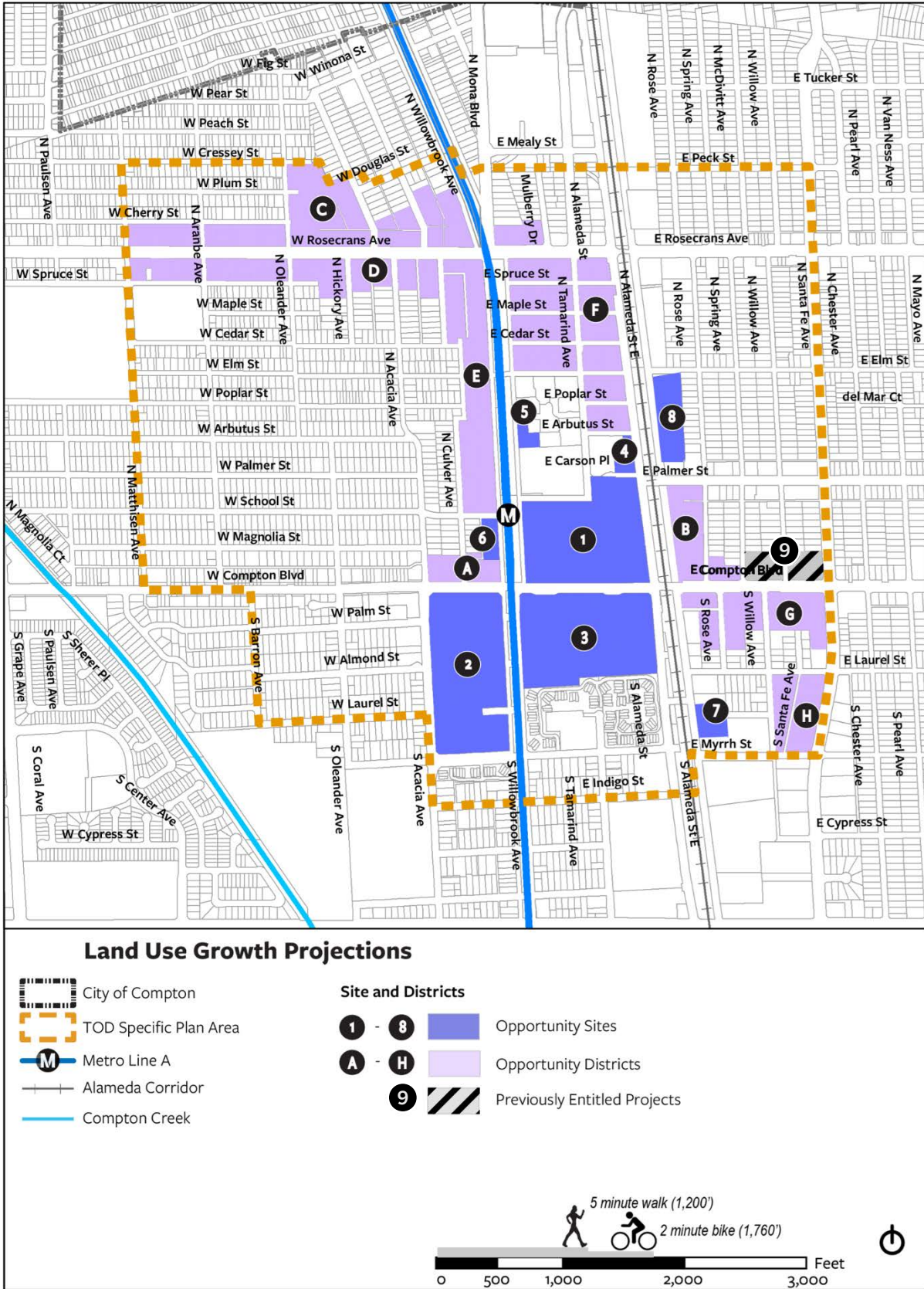


Figure 2-2. Land Use Growth Projections.

Land Use Growth Projections	Change in Land Uses					
	Public/ Institutional	Residential Market Units	Residential Affordable (units)	Retail ('000s) (sf)	Office ('000s) (sf)	Hotel (rooms)
Key Sites						
1. Renaissance Center	N	570	30	(45)	97	
2. Civic Center	Y		150	10		150
2. Towne Center	N	570	30	(18)		
3. 409 N. Alameda St. W	Y		106	4		
4. 398 N. Willowbrook Ave. E	Y		77			
5. 117-215 N. Willowbrook Ave. W	N	26	1			
6. 458 S. Alameda St. E	Y		90			
7. 600 N. Alameda St. E	Y		78			
8. 501/601 E. Compton Bl. (PREVIOUSLY ENTITLED)	N	232	58	14	40	
Total Key Sites		1,398	620	(35)	137	150
Other Opportunity Districts						
A. Compton Bl, north side, Acacia to Willowbrook	N	72	3			
B. Compton Bl, both sides, east of Alameda	N	76	4	10		
C. Rosecrans Av, north side, west of Willowbrook	N	15				
D. Rosecrans Av, south side, west of Willowbrook	Y		80			
E. Willowbrook Av W	N	43	2			
F. Rosecrans Center district	N	43	2			
G. Santa Fe/Laurel, NW corner	N	29	1			
H. Santa Fe, Laurel to Myrrh	Y		80			
Total Opportunity Districts		278	172	10		
TOTAL		1,676	792	(25)	137	150

Figure 2-3. Land Use Growth Projections Calculations.

District Descriptions

Civic Center

The Compton Civic Center is the political and symbolic heart of the City, in part because it is a product of and testament to achievements of the African-American community in Compton. Under this Plan, it will be strengthened in those roles as it invites the community and visitors in with new uses such as a hotel/cultural center, and small retail.



Commercial Core

The Commercial Core consists of two large shopping centers located in the heart of Downtown Compton and immediately proximate to the Metro station. This area, which was the original center of the city, will again become the most vibrant and active district in the region with improved restaurant, shopping and entertainment options, open space, housing, and offices in a new walkable urban pattern.



Historic Core

The Historic Core is located along a stretch of Compton Boulevard which retains more of its original built character as a pedestrian-friendly main street. This area is envisioned as a more locally-focused downtown district providing opportunities for entrepreneurs and small businesses, including arts uses.



Right photo credit: Stan Kaady.

Social Service and Institutional Area

This area features a concentration of educational, community services and religious uses, mostly oriented toward Santa Fe Avenue. The Plan envisions strengthening connections between these uses and new affordable housing and park or community garden uses located at the City Yard at 458 S. Alameda St. E. Large parking lots and vacant land also provide opportunities for community-serving development.



Alameda East Greenway Corridor

This district, currently occupied mostly by public uses and a wide, underutilized street, is envisioned overtime to become a green residential and educational corridor connected to Downtown Compton by a pedestrian and bicycle greenway replacing Alameda Street East. This district also includes Wilson Park, the only park within the generally park-poor Specific Plan area. This park is well used and includes a skate park, picnic tables, basketball courts, playground, and an indoor recreation center.



Rosecrans Center

This district is located north of the Commercial Core between Willowbrook Avenue and Alameda Street. Bisected by the Rosecrans Avenue viaduct, it is a mix of residential and industrial uses heavily impacted by industrial pollution, noise, vehicle emissions, pedestrian barriers, and poor infrastructure quality. As Downtown Compton gains strength as a center of cultural production, however, this area could receive additional interest from artisans and artists, small-scale local manufacturing with limited hours of operation, live/work development, and housing. Vacant and underutilized properties along Rosecrans Avenue present nearer-term opportunities for development.



Neighborhood Corridors

The Plan defines neighborhood corridors along Rosecrans Avenue west of Willowbrook Avenue and east of Alameda Street, and along Compton Boulevard west of Acacia Avenue. These are older strip commercial corridors with residential uses behind the mostly narrow commercial parcels. Occasional residential uses are also interspersed along the corridors. Despite high vacancies, particularly in the western portion of Compton Boulevard and the eastern portion of Rosecrans Avenue, parts of these corridors have good “bones,” with a walkable street pattern and buildings that meet the street.

The neighborhood corridors are envisioned to become pleasant, safe, and walkable corridors, combining local-serving retail with three story multifamily residential uses.



Residential Areas

The Compton Station Specific Plan’s residential areas are densely populated single- and multi-family neighborhoods. The Specific Plan envisions few changes to these residential areas; however, they will benefit from greatly increased access to services, jobs and destinations present in the Downtown.



2.3 Downtown Vision and Urban Design Framework

The vision for the Compton Station Specific Plan is to build a better downtown, transforming it into a world-class destination that the whole Compton community can enjoy. The physical framework for creating this world-class destination was guided by the following principles:

- Increase the density of development in Downtown to enable new investment, provide more customers for businesses, and create an active and vibrant center of the City
- Prioritize access to and convenience of public transit, pedestrian, and bicycling access to accommodate growth in the most sustainable fashion possible
- Activate the public realm, and create new open spaces on privately-owned land in order to enable secure and well-maintained urban environments that are open to the entire community
- Create outdoor public spaces for different kinds of activities and events, including cultural events and large gatherings
- Develop a framework for unique expressions of Compton's culture, history, and creativity.

Figures 2-4 and 2-5 portray the Urban Design Framework for Downtown Compton. The Downtown will consist of three unique districts (Civic Center, Commercial Core and Historic Core) linked to each other and to Metro through the Compton Walk. Each of these districts, and the Compton Walk, are described in detail in the sections that follow.

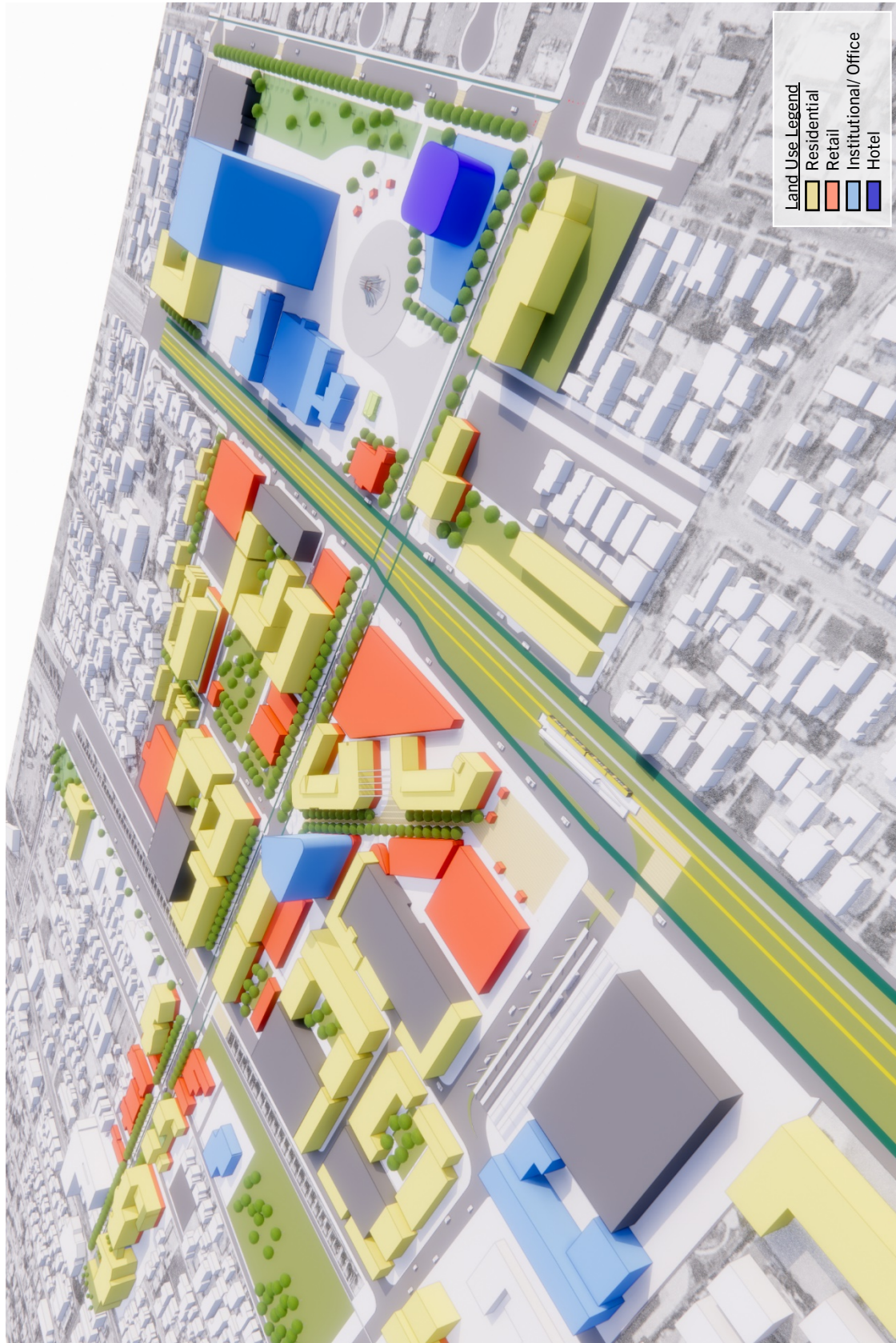


Figure 2-4. Urban Design Framework Axonometric View.



Figure 2-5. Urban Design Framework Plan View.

2.4 Civic Center

The Compton Civic Center was completed in 1977 based on a design by African-American architect Harold L. Williams. Its ten-story Courthouse and large ceremonial plaza with a monument to Martin Luther King, Jr. are iconic in the City and hold great symbolic value in recognition of Compton's role in achievements of the African-American community. Despite this important symbolic value, however, the day-to-day experience in the MLK Plaza and other Civic Center open spaces is isolating due to the extremely small numbers of people who use the spaces on a regular basis.

In order to activate the Civic Center and strengthen its role as the symbolic heart of Compton, the Specific Plan envisions a mixed-use district focused on an MLK Plaza activated by new uses, including a cultural center and hotel, coffee kiosks and restaurants, and which plays host to large events, such as concerts, demonstrations, and public celebrations. The southern portion of the Civic Center feeds this focal plaza with shared parking and neighborhood park space.

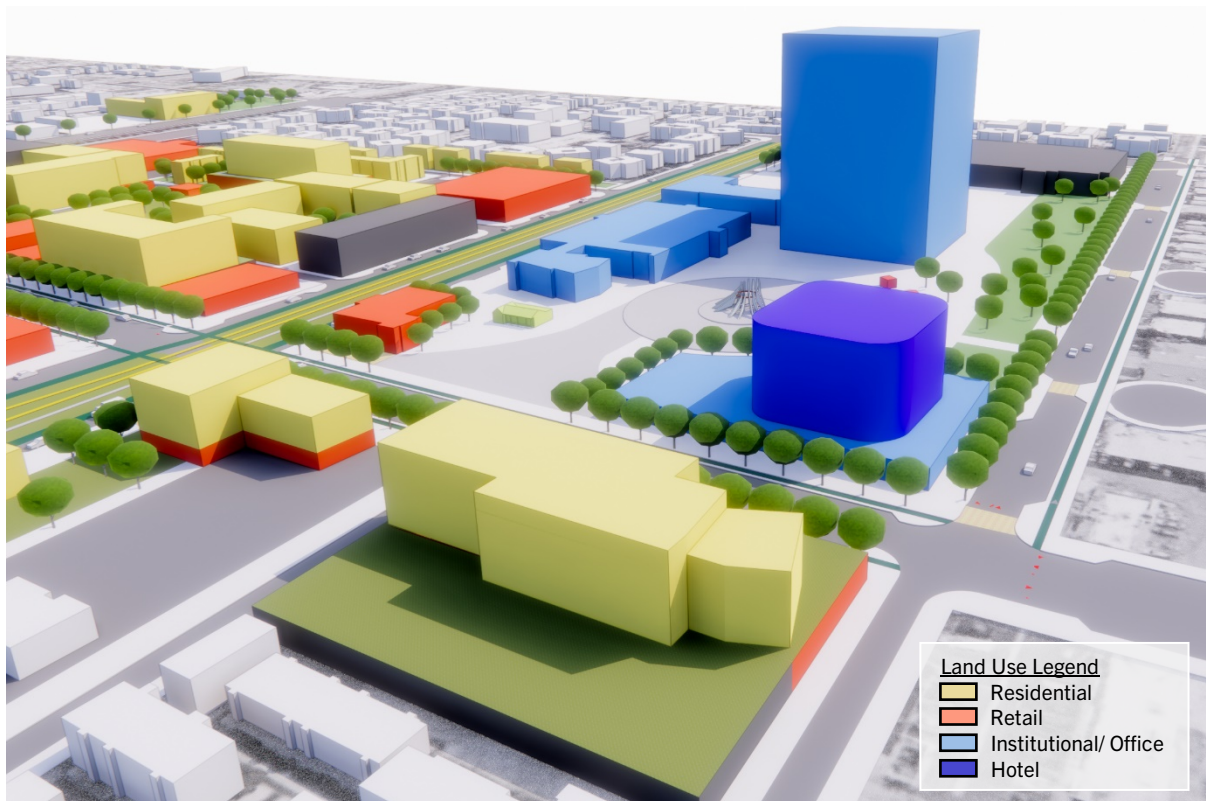






Figure 2-6. Civic Center Plan and Key Features.

Key Features:

1. Martin Luther King, Jr. Memorial Plaza programmed and equipped for large events such as concerts, demonstrations, and public celebrations.



Source: *The Heron Downtown* (right).

2. Coffee/restaurant kiosks, authorized vendors, trees, and seating added around the plaza to create a more human-scaled environment that draws people outside at different times of the day.



Source: Gary Coronado, Chron (left). Street Furniture Australia (right).

3. Potential adaptive reuse of the historic Post Office building into a more active use (such as restaurant) which helps provide a gateway and define an entry plaza into the Civic Center.

4. New library/cultural center/museum with hotel serving as an attraction for local, regional, national, and/or international tourism and a marked gateway to the Compton Walk.



New hotel, residential and retail development containing the Universal Hip-Hop Museum in South Bronx, NY. Source: L+M Development Partners.

5. Heritage House relocated to more accessible site.

6. The City will explore converting the Courthouse parking garage to shared parking, using excess daytime and nighttime capacity to accommodate the parking needs of the hotel, City Hall public meetings, and Dr. Dre Performing Arts Center events in the long term future.

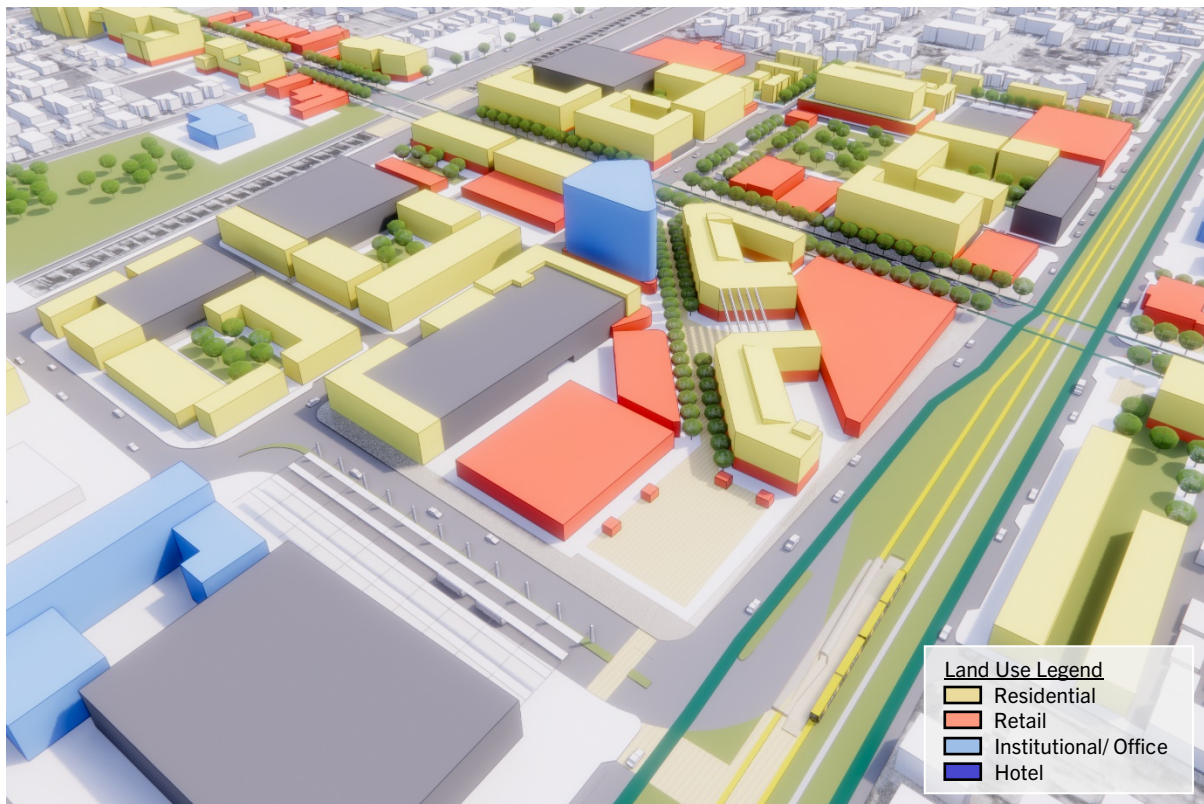
7. Strengthened neighborhood connections across Acacia Avenue, including improved crossings, narrowing of street and addition of a playground or other park-like amenity in Civic Center.

8. Connection to Compton High School and Dr. Dre Performing Arts Center.

9. Improved streetscape, parking, and bike lanes on Willowbrook Avenue (see section 2.8)

2.5 Commercial Core

Located at the Metro station and in the heart of Downtown Compton, the Commercial Core is intended to be the most dense and vibrant area of the City. This 33-acre area, currently occupied by two large shopping centers, Compton Renaissance Plaza and Compton Towne Center, is envisioned to transform over time into an active urban district and unique destination within the South Los Angeles and Gateway Cities area. The Commercial Core would continue to serve the majority of Downtown Compton's retail needs, but would arrange them along Compton Boulevard and a diagonal paseo/pedestrian mall to accentuate walkability and transit orientation and concentrate people for added safety and vibrancy. The retail uses are supplemented by upper-floor and adjacent residential units, which add eyes on the street and create the development feasibility needed to execute the project, as well as open space, structured parking, and office/institutional uses as possible.





Key Features:

1. Metro Plaza serving as an arrival gateway to downtown, sized for events such as farmers' markets, and connected to Metro station through raised crossings.



Metro Plaza (left) and raised crossing example (right).

2. Pedestrian Mall connecting the Metro station to the City Focal Intersection and flanked by active retail uses.



3. City Focal Intersection at the confluence of Compton Boulevard, Tamarind Avenue and the Pedestrian Mall with entry plaza, high visibility crosswalk, and potential location for unique tower architecture.

4. Compton Central Park providing flexible, passive space for residents and the general public.



5. New, widened streetscape along Compton Boulevard with street-fronting retail, improved lighting, outdoor dining, etc.

6. Extension of Tamarind Avenue south from realigned Carson Place/Palmer Street to new Almond Street (#13).

7. Entertainment anchor such as live or movie theater, music venue or nightclub with forecourt plaza.

8. Grocery or Big Box anchor connected to Parking Structure.



At-grade and above-grade connections from parking structures to big-box stores. Source: Google Maps.

9. Mixed-use buildings (residential or office over retail) along Compton Boulevard and key retail frontages.



10. Urban residential blocks with open space and concentrated parking.

11. Medium-density residential typologies adjacent to lower-density Country Club Drive community.

12. Alleys and new streets creating a finer-grained network of blocks.

13. New Almond Street located at front of current Compton Towne Center retail buildings, allowing for some retail buildings to be retained, with surface parking if needed.

14. District Parking Network with shared structures for retail customer/employee parking and residential parking if necessary. Structures sited and oriented to facilitate ingress and egress onto Alameda Street.

2.6 Historic Core

Located east of Alameda Street, the Historic Core is the only portion of Compton Boulevard within Downtown Compton which retains more of its early- and mid-20th century character as the main street of the community. This area is envisioned to be a more locally-focused downtown district, where creative and entrepreneurial Compton residents can find a home. The previously entitled Compton Innovation Hub, located at the east end of this district, will serve to strengthen this emphasis on creativity and entrepreneurship.



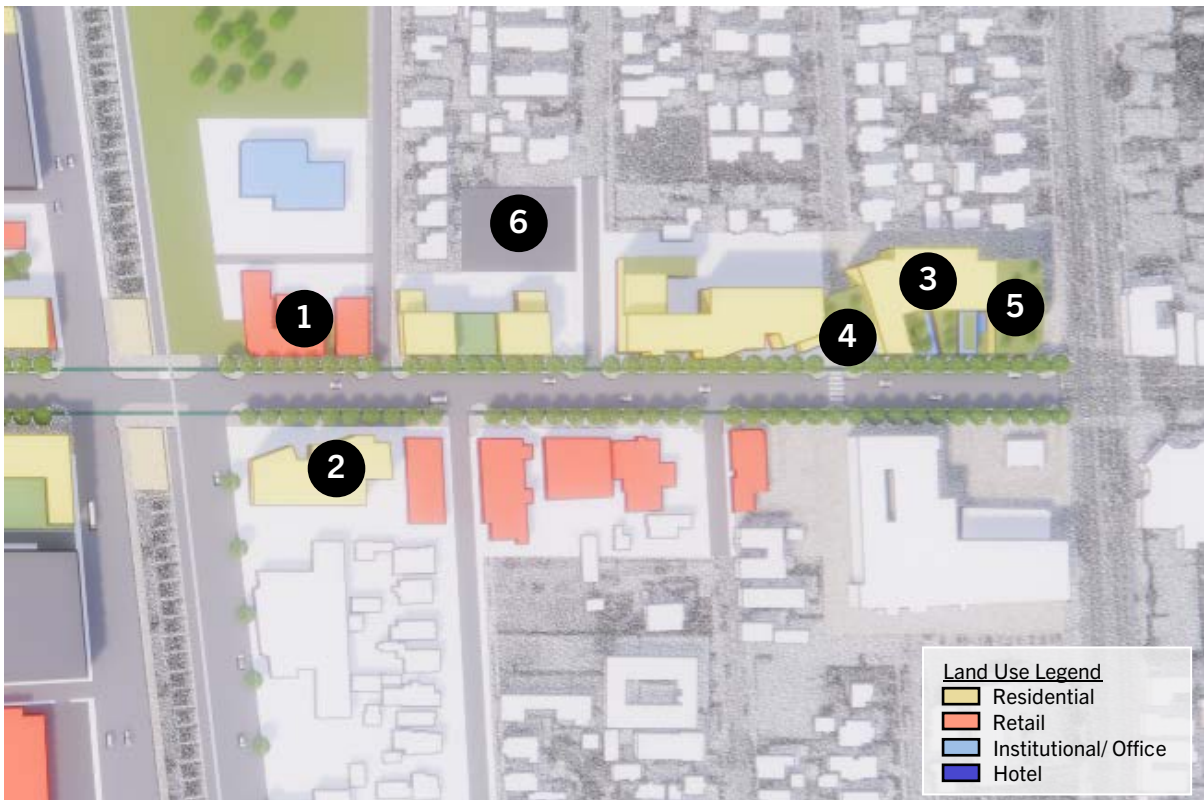
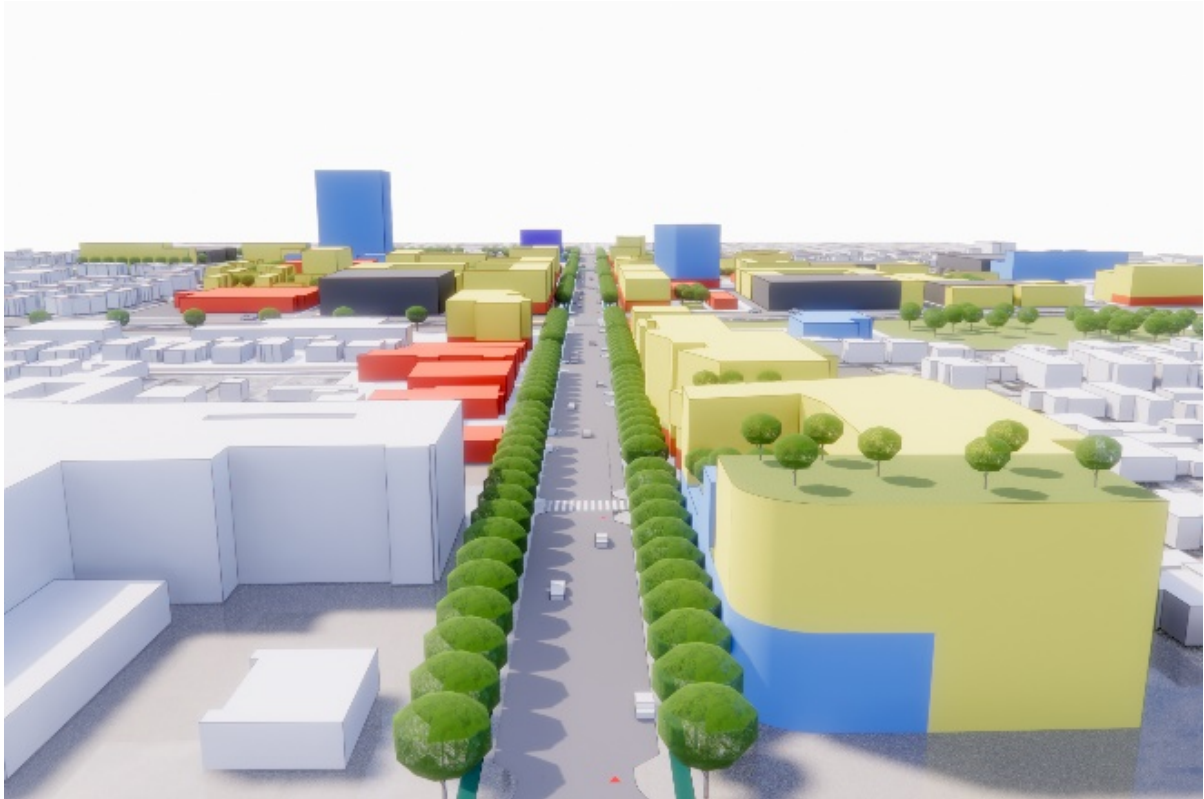


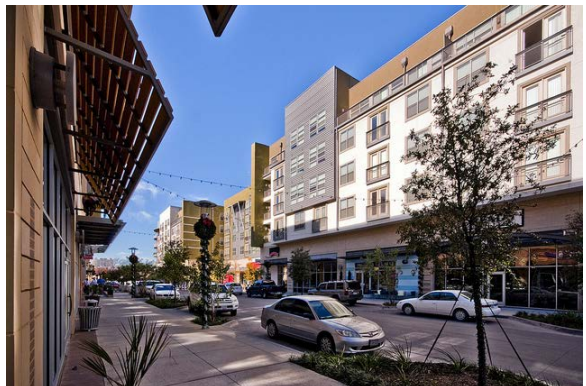
Figure 2-8. Historic Core Plan and Key Features.

Key Features:

1. Support for local businesses which contribute to the vitality of Downtown.



2. New mixed-use (residential or office over retail) development.



3. Compton Innovation Hub seven-story mixed-use project, with a large innovation hub for Compton's cultural producers.



Source: GGLO Architects



4. Willow Plaza created through closure of Willow Avenue at Compton Boulevard.



Source: GGLO Architects

5. Usable and sustainable open space created on roofs of buildings due to the small or shallow nature of many sites.

6. Public parking facilities created as opportunities arise.

2.7 The Compton Walk

This specific plan proposes that Compton Boulevard become the centerpiece of a new downtown Compton, linking the Civic Center, Commercial Core, and Historic Core districts with the Metro station through a pedestrian-friendly, activated streetscape that will take residents and visitors on a journey of discovery through cultural Compton's past, present, and future. A narrowing of Compton Boulevard's roadbed will help establish the street as a place to be, not to drive through, and free up space for additional pedestrian and active transportation uses along the corridor. The mobility concept is further described in Section 5.3.

As a nod to Compton's inspiring musical history, The Compton Walk will include a base streetscape patterning and design intended to resemble a blank musical 'score sheet', upon which the local community will be encouraged to compose and share their own unique 'songs' and expressions about Compton. The experiences along the Walk will be created, maintained, and updated over time by local artists and business owners, expressing the City of Compton's contributions to music, sports, politics, theater, visual arts, and fields yet undefined. It will serve as a major attraction to those who wish to come to appreciate and explore the City's culture.

Figure 2-9 illustrates diagrammatically how a range of programming and activities can be composed to generate a unique and evolving range of experiences, similar to how notes can be combined to make a song. Punctuating this primary pathway of experiences will be a rhythm of streetscape 'Places' associated with adjacent private development and other civic open spaces that will seek to promote additional moments of connection, discovery, learning, history, cultural expression, and entertainment.

Figure 2-10 shows the overall extents and routing of the Compton Walk. Key districts and civic spaces will be connected and a regular placement and hierarchy of programming and space types will promote exploration and discovery.

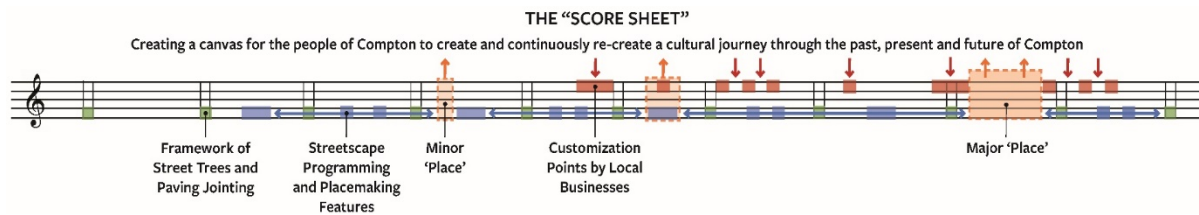


Figure 2-9. Streetscape 'Score Sheet' Programming Concept.

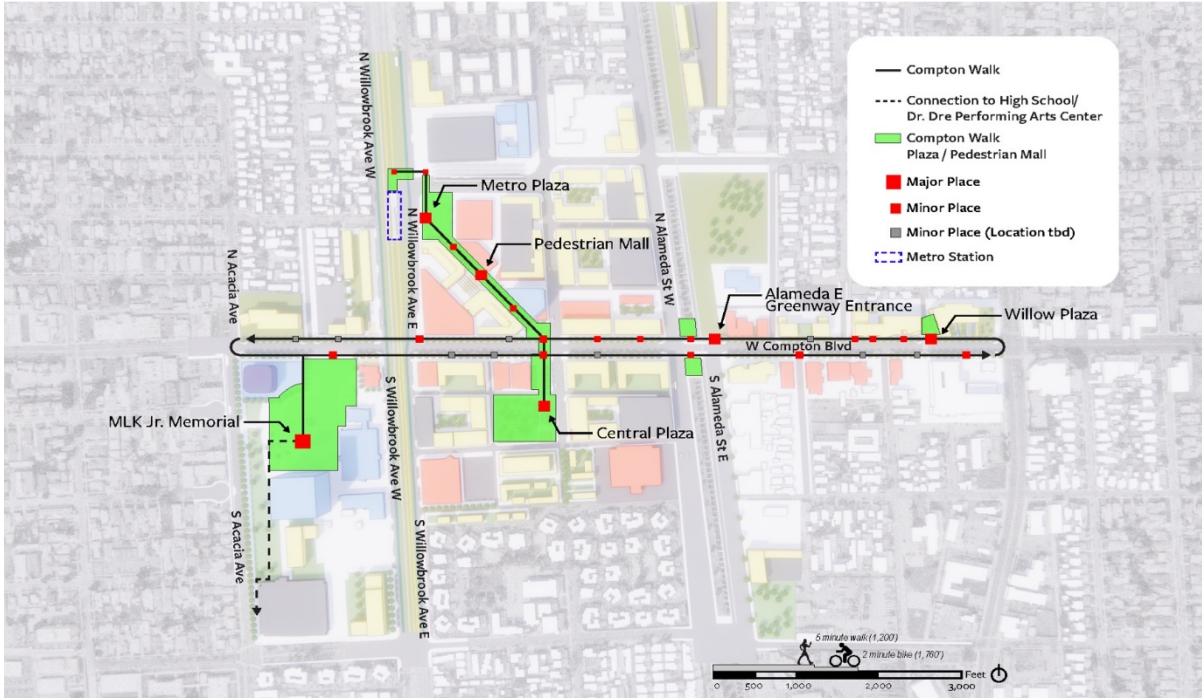


Figure 2-10. Overall Compton Walk Routing and 'Places' Concept.

Base Streetscape Improvements.

These improvements are to be implemented as a condition of development in keeping with the Specific Plan standards, reference Section 5.3.

These improvements would include hardscape upgrades to the sidewalks, the planting of street trees, and the selection and placement of essential street furniture. The goal of these improvements will be to harmonize the streetscape to a consistent base look and feel and provide the streetscape components needed to create a more usable and comfortable physical space. They would create a solid base streetscape condition that could be easily implemented and that a later, more extensive design process could build on with further visioning and locally crafted expression. **Section 5.3** provides initial standards for the build-out of the base streetscape improvements.

Streetscape Customization: Historic, Art and Placemaking Features.

These improvements are to be implemented by the City, or by the Developer, through a design review process as described in **Section 3.10**. Reference Sections 4.14 and 4.15 for proposed placemaking and private development interfacing guidelines.

These streetscape improvements would include, but not be limited to, the addition of a range of new expressive art and cultural elements such as sidewalk art, sculptural art elements, decorative hardscape paving, a range of lighting and multimedia effects, historical markers, a hierarchy of signage including information signage, streetscape structures, a district branding approach and elements, and other features TBD. The goal of this stage will be to undertake a design process that will enable Compton stakeholders to explore and add their own storytelling

layers to a Compton Walk experience. Substantive engagement with the local community, particularly the arts and historic communities, will ensure that the Walk is expressive of what the voices of Compton want to say. Involvement with a range of design specialists and creative professionals during the visioning process will play an important role in crafting a vision and experience that is nuanced and full of discovery.

Building on the base 'Score Sheet' streetscape improvements, the Compton community will be encouraged to compose its own melody of artistic, historical, and interactive elements which will be the focus of this journey through cultural Compton. The process to choose and develop the elements of the streetscape will need to be inclusive, weaving different elements and layers together to create a cohesive song of experiences.



Figure 2-11. Compton Walk Concept

2.8 Willowbrook Avenue

Willowbrook Avenue (actually two streets: Willowbrook Avenue West and Willowbrook Avenue East) is the major access pathway to the Metro station; yet today it functions as a barrier in the urban environment which is confusing to new users and unsafe for pedestrians, especially at intersections. The preferred solution is to convert it into a couplet of one-way streets, with bicycle facilities on both sides helping residents on both sides of the tracks access the station. The COMPTON Letters Plaza will be removed to re-establish vehicular access. Additional greenery will also be added to the couplet. See Section 5.4 for further detail regarding the redesign of Willowbrook Avenue.

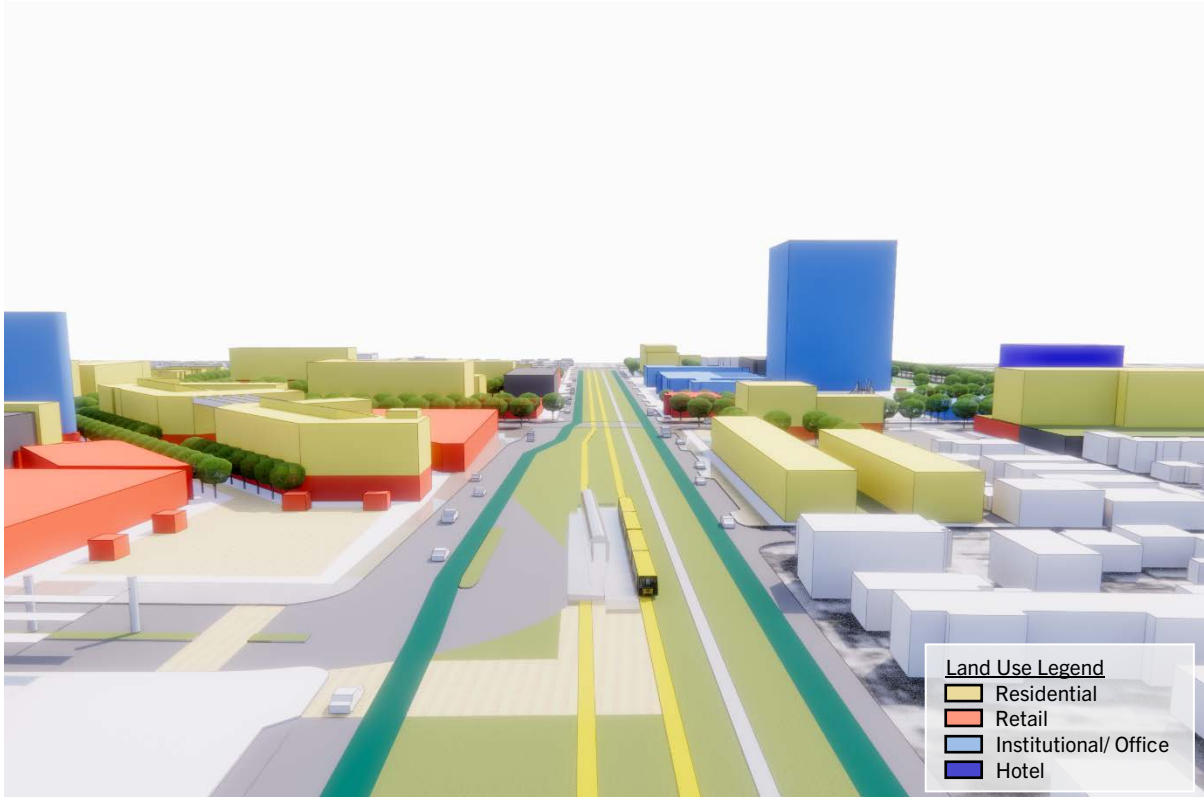




Figure 2-12. Willowbrook Avenue Plan and Key Features.

Key Features:

1. Simplified circulation pattern with Willowbrook Avenue West restricted to one-way traffic northbound and Willowbrook Avenue East to one-way traffic southbound.
2. Martin Luther King, Jr. bus station preserved, with minimal effect on bus access and operation.
3. Willowbrook Avenue East reconnected through the COMPTON Letters Plaza to enable direct access from Rosecrans Avenue to the Commercial Core.
4. New low-stress north-south bicycle facilities and connections to regional systems.
5. Raised crossings connecting Metro station entrance to the Metro plaza on the southeast corner of Willowbrook Avenue East and Palmer Street.
6. Street tree bulbs added to improve pedestrian comfort along the street.
7. Simplified and safer Compton Boulevard and Willowbrook Avenue intersection.
8. Rear exit through Courthouse driveway for Sheriff's Department vehicles exiting to the South.
9. Additional parking opportunities provided along Compton Boulevard and Acacia Avenue which can provide convenient access for those negatively affected by the elimination of southbound travel lanes along Willowbrook Avenue West.

2.9 Alameda East

Alameda East between Rosecrans Avenue and Compton Boulevard is a little-used roadway which is intended to convert into a greenway, as described in Section 2.2. The greenway can provide much-needed green space, provide safe bicycle and pedestrian access from nearby neighborhoods to Wilson Park and Downtown Compton, and create a suitable environment for new residential uses to be built along its east side. Above all, the greenway would create a sorely lacking recreational amenity for the central Compton population and improve public health.

The Alameda East Greenway would close Alameda Street East as a vehicular roadway at the City's discretion. This would need to be done in conjunction with the extension of the Rosecrans Avenue frontage roads across the Alameda Corridor in order to ensure access between Rosecrans Avenue and Alameda Street, and only at the pace that redevelopment proceeds along the street so that vehicular access to properties is not affected. See Section 5.5 for further detail regarding the redesign of Alameda East, and for alternative designs should the roadway not be able to be closed.

Key Features:

1. Greenway and road closure designed as linear park, with paved path for cyclists and skateboarders, decomposed granite for joggers, exercise stations, shade, and stormwater treatment elements.



2. Wilson Park (playground, basketball courts, skate park, rec center, etc.)

3. Extension of narrowed greenway, with narrowed street, or sidewalk greening south of Compton Boulevard with tree wells in parking lane.

4. Community garden and path termination built with affordable housing development on Corporate Yard.



Figure 2-13. Alameda East Greenway Plan and Key Features

Chapter 3: Zoning, Development, and Design Standards

3.1 Administration and Procedures

1. Purpose and Intent.

This Administration and Procedures Section sets forth the processes used for the application, review, and decision-making for land development and use requests within the Specific Plan. It is required by this Specific Plan that any developer, builder, property owner, or other authorized agent seeking to establish a use and/or develop pursuant to this Specific Plan will submit an application processed in an expeditious manner with administrative approvals where allowed by these Administrative Procedures. Applications that deviate from the standard provisions herein, will require discretionary review and action.

2. Applicability.

- A. Requirements for structures and land uses. No building permit shall be issued by the City unless the proposed project complies with all applicable provisions of this Chapter, including applicable findings, conditions of approval, and all other applicable provisions of law. Where this Specific Plan imposes a greater restriction upon the use of buildings or land or requires larger open spaces than are imposed or required by the Municipal Code or other ordinances, rules, regulations or by easements, covenants or agreements, the provisions of this Specific Plan shall prevail.
- B. Legal Parcel. The site of a proposed land use, development, modification, or other improvement subject to this Specific Plan shall be on a parcel(s) legally created in compliance with the Subdivision Map Act and the City's Subdivision Regulations.
- C. Minimum requirements. The provisions of this Chapter are minimum requirements for the protection and promotion of the public health, safety, and general welfare.
- D. Effect on Existing Development and Land Uses. Development and/ or use(s) legally existing as of the adoption of this Chapter shall comply with Section 30-23 (Nonconforming Buildings and Uses) of the City Zoning Code.
- E. Permits and Procedures. Permits and procedures legally existing in City Zoning Code as of the adoption of this Chapter shall continue to apply, in addition to other permits and approvals listed in this Section.
- F. Nonapplicable Zones. No changes shall be made to zones not specified within this Specific Plan. Citywide standards apply to zones not specified within this Specific Plan.

3. Review Authority.

- A. This Specific Plan shall be administered by the City Council, Planning Commission, and the Community Development Department Director. Table 4.1 (Designated Approving Authority) identifies the review authority established for the code.

4. Specific Plan Interpretation.

- A. The Community Development Department Director shall interpret the provisions of the Specific Plan. Requests for a Specific Plan Interpretation shall be filed with the Community Development Department Director. The Community Development Department Director shall respond in writing to written requests for Specific Plan Interpretations within 30 days from the date the request has been submitted. The Community Development Department Director may issue an official interpretation or refer any issue of interpretation to the Planning Commission for its determination.

- B. Official interpretations shall be written and shall quote the provisions of this Specific Plan being interpreted, and the applicability in the particular or general circumstances that caused the need for interpretations, and the determination. Written interpretations shall become a part of the Specific Plan, as a separate appendix. The Community Development Department Director will maintain a complete record of all official interpretations to this Specific Plan, indexed by the number of the Chapter or Section that is the subject of the interpretation.

5. Administrative Relief.

- I. Purpose. The purpose of this Section is to enable the Community Development Department Director to approve minor deviations from the development standards of this Chapter. An authorization to approve Administrative Relief does not extend to making any changes in the uses permitted in any zone.
- II. Applicability. The Community Development Department Director may allow Administrative Relief up to a maximum of 20 percent of any measurable standard of this Chapter, or the amount prescribed for the items specified in Figure 3-2 (Type of Administrative Relief Allowed), whichever is most restrictive. The Community Development Department Director may allow Administrative Relief only after first making the findings specified in Subsection 3.1.5.V (Findings and Decision) and Figure 3-2 (Type of Administrative Relief Allowed).
- III. Review Authority. The Community Development Department Director may approve or deny an application for Administrative Relief. If outside the parameters, it cannot be approved.
- IV. Application Review.
 - I. The Community Development Department Director, in compliance with Subsection 3.1.5.II, shall review the application for Administrative Relief and issue a decision within 15 days.
 - II. A public hearing shall not be required for the decision on Administrative Relief.
- V. Findings and Decision. An application for Administrative Relief may be approved subject to conditions or denied by the review authority. The review authority may impose any conditions deemed reasonable and necessary to ensure that the approval will comply with the findings required by this Section. The review authority may approve Administrative Relief only after first making all of the following findings in writing.
 - I. The development meets all other standards and requirements in this Specific Plan.
 - II. There are special circumstances uniquely applicable to the property that cannot be resolved by a redesign of the project, such as its size, shape, topography, location, or surroundings;
 - III. Approving Administrative Relief will not grant special privileges inconsistent with the limitations upon other properties in the vicinity and the zone in which the property is located;
 - IV. The special circumstances uniquely applicable to the property are not self-imposed by any person presently having an interest in the property; and,
 - V. The requested Administrative Relief will not allow the establishment of a use that is not otherwise permitted in the zone.
 - VI. The subject site cannot be otherwise developed through a redesign or reduction in the size of the project.
- VII. Appeal. The applicant may file a written appeal with the Planning Commission within 15 days after the Community Development Department Director decision. The Planning Commission shall hear the appeal within 30 days or at its next succeeding regular meeting,

unless the applicant consents to a continuance. Upon conclusion of the hearing, the Planning Commission shall, within 7 days, declare its decision in writing based upon the testimony and documents produced before it. It may sustain, modify, reject, or overrule, any action of the Community Development Department Director provided such action is not inconsistent with the provisions of this Chapter or any other applicable law or statute. The appeal fee shall be the same as that of an appeal to the City Council, and the procedures followed shall be the same as in Municipal Code section 30-26.5 (i) – (k), except that the Planning Commission shall hear the appeal.

Designated Approving Authority				
Type of Permit or Action	Reference	Review/Approving Authority		
		Community Development Department Director	Planning Commission	City Council
Administrative Actions				
Specific Plan Interpretation	Section 3.1.4 (Specific Plan Interpretation)	Decision	Appeal	N/A
Use Determination	Section 3.1.3 (Use Determination)	Decision	Appeal	N/A
Development Permits and Approvals				
Administrative Relief	Section 3.1.5 (Administrative Relief)	Decision	Appeal	N/A
Conditional Use Permit	Section 3.1.6 (Conditional Use Permit)	Recommend	Decision	Appeal
Design Review Tier 1	Section 3.1.7 (Design Review)	Decision	Appeal	N/A
Design Review Tier 2	Section 3.1.7 (Design Review)	Recommend	Decision	Appeal
Minor Use Permit	Section 3.1.8 (Minor Use Permits)	Decision	Appeal	N/A
Subdivisions	Section 3.1.9 (Subdivisions)	-	Recommend	Decision
Temporary Use Permit	Section 3.1.10 (Temporary Use Permit)	Decision	Appeal	-
Variance	Section 3.1.11 (Variance)	Recommend	Decision	Appeal

Figure 3-1. Designated Approving Authority.

Type of Administrative Relief Allowed			
#	Type of Administrative Relief	Additional Required Findings	Maximum Modification
1	A reduction in the minimum amount of building placed within the façade zone.	The building and site design are consistent with the intent of the zone and all other applicable standards of the zone; and may not be combined with any reduction to frontage requirements.	10%
2	An increase of the maximum required setback.	Existing building(s) on adjacent lots on the same block face is/are greater than the maximum setback; and the variation will allow the proposed project to be more closely aligned with the adjacent building(s).	5 ft.
3	An increase or decrease in the standards for building frontages.	The building is consistent with the intent of the zone and all other applicable standards of the zone.	20%
4	An increase in allowable building height.	The building is consistent with the intent of the zone and all other applicable standards of the zone; the portion of the building which exceeds the height limit is at least matched by other portions which are below the height limit by the same amount as the increase requested and the increase accommodates architectural or design features as part of the proposed project.	Up to a maximum of 15 feet over and no more than 30% of the building footprint.
5	An increase of the allowed maximum height of a fence or wall.	The use of the lot is consistent with the intent of the zone and all other applicable standards of the zone; and/or the height increase is justified by topographic considerations	20%
6	A reduction in the minimum number of required bicycle parking spaces.	There are demonstrated sufficient bicycle parking opportunities within 300 feet of the project location.	40%
7	An increase in the allowed height and area of a permanent sign.	The sign is otherwise consistent with all applicable sign regulations.	15%
8	A reasonable deviation from the prescribed standards where necessary to install features that facilitate access and mobility of persons with disabilities or with limited mobility.	The building is consistent with the intent of the zone and all other applicable standards of the zone.	Determined on a case-by- case basis

Figure 3-2. Type of Administrative Relief Allowed.

6. Conditional Use Permit.

Applications for uses set forth in Figures 3-4, 3-7, 3-10, 3-13, and 3-16 (Permitted Uses) of the Specific Plan that require approval of a conditional use permit shall be processed in accordance with the provisions of Section 30-26 (Conditional Use Permits) of the City Zoning Code.

7. Design Review.

- A. Purpose. Design Review is intended to implement urban design goals, policies, development standards, and design guidelines within this Specific Plan. Design Review is intended to ensure that the design of new construction and additions is contextual and supports design excellence in Compton.
- B. Limitations. The standard conditions imposed upon approval of an application during Design Review will not be more restrictive than those prescribed by applicable zoning district regulations or other applicable entitlements per this Specific Plan.
- C. Exemptions. Alterations, additions, and repairs that do not change the exterior appearance of a structure, including replacement in kind of existing features do not require Design Review. To be considered “replacement in kind,” the features must reasonably match the design, profile, material, and general appearance of the originals. Revised plans of the change shall be filed with the Community Development Department. Residential-only projects with four or less units are not subject to Design Review, but still will require Architectural Review Board review.
- D. Tier 1 Administrative Design Review.
 - I. Applicability. This review process applies to the following development proposals:
 - a. Temporary signs and other signs set forth in Section 3.13 (Signage Standards).
 - b. New construction or additions that feature more than five residential units and less than sixty residential units. New residential/mixed-use construction or addition projects with up to sixty units, including up to 8,000 square feet of retail.
 - c. Any mixed-use or nonresidential project that creates less than 400 daily vehicular trips as established by a traffic generation memo prepared by a professional engineer (PE) or traffic engineer (TE).
100% affordable housing projects of any size, with maximum income levels of 80% low income and 20% moderate income units, with up to 8,000 square feet of supportive retail.
 - II. Review Authority. The Community Development Department Director will conduct Tier I Design Review and make decisions to approve the application or impose conditions upon the approval of the application. The Community Development Department Director may refer any such project for a public hearing before the Planning Commission to receive comment on the project’s design, but not to approve or deny the project, if the Director determines it has special significance.
 - III. Review Process. The fees and process shall be equivalent to the City’s Architectural Review Board application.
 - IV. Required Findings. In acting to approve or conditionally approve an application for the Tier 1 Design Review, the Community Development Department Director shall make the following findings:
 - a. That the project is consistent with the applicable development standards of the Specific Plan and any design guidelines of this Specific Plan as deemed applicable by the Community Development Department Director; and

b. That the project implements applicable goals and policies of the Compton General Plan, as deemed applicable by the Community Development Department Director.

E. Tier 2 Public Hearing Design Review

- I. Applicability. Tier 2 Public Hearing Design Review is required for any new construction, addition, exterior modification, or renovation that does not qualify for Tier 1 review.
- II. Traffic Study Required. All projects subject to Tier 2 review shall prepare a focused traffic analysis which includes evaluation of all project driveway access points and potential effects at the nearest signalized intersection (in terms of Level of Service and Queuing). The focused traffic analysis should be prepared by a licensed professional engineer (PE) or traffic engineer (TE). The Public Works Director and/or City Traffic Engineer may, at his or her discretion, require traffic analysis at additional locations based on project size, type, and location.
- III. Review Authority. The Planning Commission has authority under Tier 2 Public Hearing Design Review. The Planning Commission is authorized to act upon the design and approve the application or impose conditions upon the approval of the application. The Planning Commission is intended to review each project for compliance with the urban design goals, policies, development standards, and design guidelines of this Specific Plan.
- IV. Required Findings. In acting to approve or conditionally approve a Tier 2 Public Hearing Design Review application, the Planning Commission shall make the following findings:
 - a. That the project is consistent with the applicable Compton General Plan policies, Specific Plan standards, and Specific Plan design guidelines.
 - b. That the project furthers the goals, actions, and Urban Design Framework of the Specific Plan.
 - c. That the project provides appropriate amenities for the target population, including public amenities if the project is commercial or mixed-use.
- V. Application. In the absence of a specialized application form, the applicant shall use the application form for a Conditional Use Permit.

8. Minor Use Permit.

- A. Purpose. A Minor Use Permit (MUP) provides a process for reviewing land uses that may be appropriate in the applicable zone, but whose effects on a site and its surroundings cannot be determined before being proposed for a specific site. The purpose of a Minor Use Permit is to provide sufficient flexibility and to reduce processing times for projects that are consistent with and further the objectives of the Specific Plan.
- B. Applicability. A MUP is required to authorize land uses identified by Sections 3.3 through 3.7 as being allowed subject to the approval of a MUP in the applicable zone.
- C. Review Authority. The application for a MUP shall be reviewed and approved or denied by the Community Development Department Director. The Community Development Department Director may choose to refer any MUP application to the Planning Commission for review and final decision.
- D. Application Filing, Processing, and Review.
 - I. Application filing and processing. The application shall be filed with the Compton Community Development Department in accordance with this Section, include the information and materials specified in the most up-to-date Department handout for a Minor Use Permit, together with the required fee. It is the

responsibility of the applicant to provide evidence in support of the findings required by Subsection 3.1.8.F (Findings and Decision), below. The fee for the MUP shall be the same ARB fee established for new commercial/industrial projects.

- II. Application review. Each application shall be reviewed by the Community Development Department Director to ensure that the proposal complies with all applicable requirements of this Chapter.
- E. Public Hearing and Noticing.
- I. Public hearing. A public hearing is not required for a Minor Use Permit decision.
 - II. Notice. Before a decision on a Minor Use Permit, the Department shall provide notice by posting the site, City Hall, and the City of Compton website for 10 days prior to taking action.
- F. Finding and Decision.
- I. An application for a Minor Use Permit may be approved subject to conditions or denied by the review authority. The review authority may impose any conditions or time limits deemed reasonable and necessary to ensure that the approval will comply with the findings required by this Section.
 - II. The review authority may approve a Minor Use Permit only after first making all of the following findings in writing:
 - a. The proposed use is consistent with the General Plan and Specific Plan;
 - b. The proposed use is allowed with an MUP within the applicable zone and complies with all other applicable provisions of the City Zoning Code and this Chapter;
 - c. The design, location, size, and operating characteristics of the proposed activity will be compatible with the existing and future land uses in the vicinity;
 - d. The site is physically suitable in terms of:
 - The provision of public and emergency vehicle (e.g., fire and medical) access;
 - Public protection services (e.g., fire protection, police protection, etc.);
 - The provision of utilities (e.g., potable water, schools, solid waste collection and disposal, storm drainage, wastewater collection, treatment, and disposal, etc.); and
 - The type, density, and intensity of use being proposed will not endanger, jeopardize, or otherwise constitute a hazard to the public health, safety, or general welfare, or be materially detrimental or injurious to the improvements, persons, property, or uses in the vicinity and zone in which the property is located.
- G. Appeal. The applicant may file a written appeal with the Planning Commission within 15 days after the Community Development Department Director decision. The appeal fee shall be the same as the fee established for City Council appeals. The Planning Commission shall hear the appeal within 60 days, unless the applicant consents to a continuance. Upon conclusion of the hearing, the Planning Commission shall, within 15 days, declare its finding based upon the testimony and documents produced before it. It may sustain, modify, reject, or overrule, any action of the Community Development Department Director provided such action is not inconsistent with the provisions of this Chapter or any other applicable law or statute. The appeal fee shall be the same as that of an appeal to the City Council, and the procedures followed shall be the same as in

Municipal Code section 30-26.5 (i) – (k), except that the Planning Commission shall hear the appeal.

H. Permit to Run with the Land. A MUP approved in compliance with the provisions of this Section shall continue to be valid upon a change of ownership of the business, parcel, service, or structure.

I. Post Decision Procedures.

I. Duration and Revocation.

a. A MUP becomes null and void if not used within 12 months following its effective date, or within a shorter time specifically prescribed as a condition of the MUP, or at the expiration of an associated development permit if that occurs at a later time. The Community Development Department Director may, without a hearing, extend the time for a maximum period of one additional 12-month period only, upon application and fee filed with the Planning Department before the expiration of the 12-month or shorter time period.

b. In any case where the conditions of a MUP have not been or are not being complied with, the Community Development Department Director shall give written notice to the permittee of intention to revoke or modify the MUP and shall set a date for a public hearing upon the proposed revocation or modification. The notice shall be served on the MUP applicant and the owner of the subject property by mailing the notice to the owner at the address shown on the last equalized assessment roll at least 10 days before the date of the hearing, and specify the date, time, and place when and where it will be held. Notice of the intent to revoke the MUP shall specify the actions causing revocation/modification and list previous City actions taken to achieve compliance. Following a hearing with the Planning Commission, and if the Commission finds that there is good cause therefore, the Commission may revoke or modify the MUP.

c. If a use granted under a MUP is abandoned for a period of 6 months, the MUP shall expire, without any further City action.

II. Penalty for Violations.

a. The violation by any person of any provision or condition of a MUP granted under the terms of this Section is an infraction, punishable in compliance with Section 30-38 (Enforcement Procedure) of the City Zoning Code.

b. Each person is guilty of a separate offense for each and every day during any portion of which a violation is committed, continued, or permitted, and shall be punished accordingly.

9. Subdivisions.

A. The regulations set forth in Chapter XXVIII (Land Subdivision) of the Compton Municipal Code, shall apply to all divisions of land hereafter made of property within the Specific Plan.

10. Temporary Use Permit.

A. Applicability. Use Permits (TUP) shall be subject to the provisions contained herein. TUPs shall be subject to the current Administrative Review process and fee.

B. Review Authority. The application for a TUP shall be reviewed and approved or denied by the Community Development Department Director.

C. Appeals. Community Development Department Director decision regarding Temporary

Use Permits shall be subject to appeal to the Planning Commission, whose decision shall be final. The appeal fee shall be the same as that of an appeal to the City Council, and the procedures followed shall be the same as in Municipal Code section 30-26.5 (i) – (k), except that the Planning Commission shall hear the appeal.

- D. Exempt Temporary Uses. The following minor and limited duration temporary uses are exempt from the requirement for a Temporary Use Permit:
- I. City sponsored Events: any temporary activities sponsored by the city, on nonresidential property. Any temporary activities sponsored by the city, unless the Community Development Department Director determines a TUP is necessary to effectuate the purposes of this Chapter.
 - II. Emergency Facilities. Emergency public health and safety needs/land use activities.
 - III. On-site Construction Yards. On-site contractors' construction yards, including temporary trailers and storage of equipment, in conjunction with an approved construction project on the same site. The construction yard shall be removed within 30 days of issuance of a Certificate of Occupancy.
- E. Temporary Use Permit Required. Temporary Use Permit shall be required for the following types of temporary uses:
- I. Car Washes. Car washes conducted by a qualifying sponsoring organization on non-residential properties. Sponsorship shall be limited to educational, fraternal, religious, or service organizations directly engaged in civic or charitable efforts, or to tax exempt organizations in compliance with 501(c) of the Federal Internal Revenue Code. Temporary car washes shall not occur on a site more than four times per calendar year and may not operate for a continuous period of more than 12 hours or for more than two contiguous days.
 - II. Commercial Filming. The temporary use of an approved site for the filming of commercials, movies, videos, etc. The Community Development Department Director shall make the additional finding that the approval would not result in a frequency of uses likely to create incompatibility between the temporary filming activity and the surrounding neighborhood. Filming shall be limited to no more than 15 consecutive days.
 - III. Events. Events including the following:
 - a. Arts and crafts shows, auctions, civic and community events, fairs, farmers' markets, festivals, flea markets, food events, recreation events, rummage sales, second-hand sales, street fairs, and swap meets. Temporary Use Permits can be for up to 12 months.
 - b. Outdoor meetings or group activities for seven consecutive days or less, within a 90-day period.
 - VI. Off-site Construction Yards. Off-site contractors' construction yards, in conjunction with an approved construction project within the Specific Plan area. The permit shall expire and the construction yard shall be removed within 30 days of Certificate of Occupancy for the project in question.
 - VII. Residence. A temporary caretaker quarters during the construction of a subdivision, multi-family, or nonresidential project within the Specific Plan area. The permit may be approved for a specified duration, or upon expiration of the Building Permit, whichever first occurs.
 - VIII. Seasonal sales of pumpkins and Christmas trees or other national holiday sales for a period not longer than 45 days.
 - IX. Amusement rides, carnivals, circuses, concerts, live entertainment, outdoor entertainment/sporting events, and tent revivals for a period not longer than 30

days.

- X. Onsite Temporary Work/Office Trailer. A trailer or modular office may be used as a temporary work site for employees of a business during construction of a subdivision or other development project within the Specific Plan area when a valid Building Permit is in force.
- XI. Temporary uses otherwise regulated by this Chapter or similar temporary uses. Similar temporary uses which, in the opinion of the Community Development Department Director, are compatible with the Specific Plan and surrounding land uses, and are necessary because of unusual or unique circumstances beyond the control of the applicant.

11. Variance.

- A. Applications for a variances to any standards in this Chapter shall be processed in accordance with the provisions of Section 30-27 (Variances) of the City Zoning Code.

3.2 Zoning Districts

Figure 3-3 maps the existing and proposed zoning districts within the Compton Station Specific Plan Area. The standards and procedures for the RL, RM, RH and MH zones are as defined in the appropriate sections of the Compton Municipal Code.

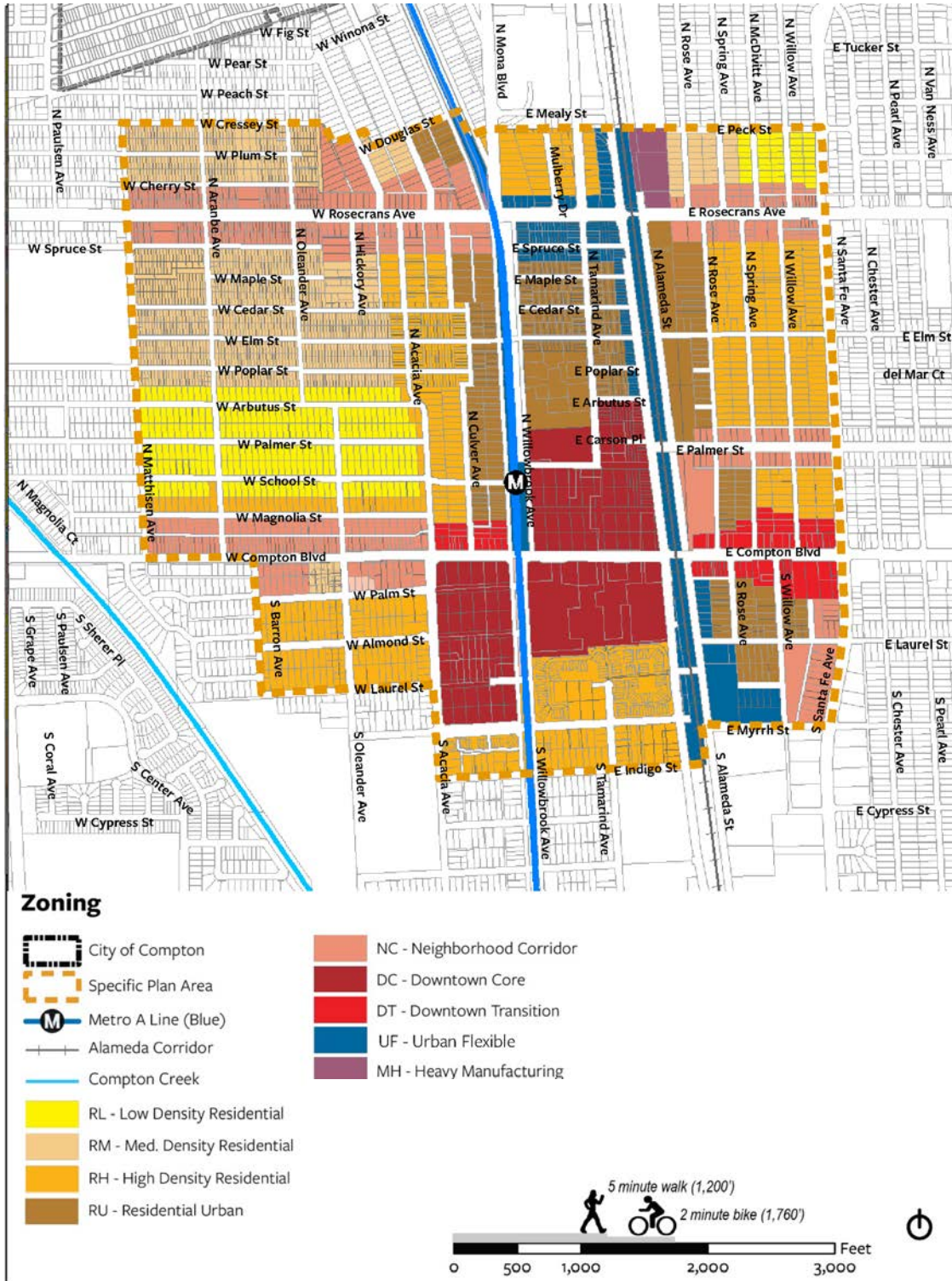


Figure 3-3. Zoning Map.

3.3 Downtown Core (DC) Zone

1. Land Use Regulations.

Figure 3-4 (Permitted Uses) describes the land use regulations for the Downtown Core Zone. In addition, the Compton Walk Front, within the Downtown Core Zone, features its own column of permitted land uses in Figure 3-4. The land use permitted for each zone are established as follows: Tier 1 or 2 review only.

- A. **“P”** – Uses permitted by right that require no discretionary review if in compliance with all standards
- B. **“M”** – Uses subject to a Minor Use Permit following administrative review by the Community Development Department Director
- C. **“C”** – Uses subject to a Conditional Use Permit following discretionary review and public hearing by the Planning Commission
- D. **“T”** – Uses subject to a Temporary Use Permit following administrative review by the Community Development Department Director

2. Establishment of Permitted Uses.

- A. Any of the land uses permitted in Figure 3-4 (Permitted Uses), may be established on any lot within the Downtown Core zone, subject to the permit requirement listed in the Figure 3-4 (Permitted Uses), and in compliance with all applicable standards of this Chapter.
- B. If a proposed use is not listed in Figure 3-4 (Permitted Uses), the use is not permitted unless the Community Development Department Director determines in writing that the proposed use is substantially similar to a use specified as a permitted or conditional use in that zone in compliance with Subsection 3 (Use Determination).

3. Use Determination.

- A. The Community Development Department Director shall have the authority to make an administrative determination as to whether a specific use is substantially similar to one of the listed used in Figure 3-4 (Permitted Uses). The Community Development Department Director may at his or her discretion refer the determination to the Planning Commission. In making a determination that a proposed use is or is not similar to those listed, the Community Development Department Director or Planning Commission shall consider:
 - I. Whether the characteristics of, and activities associated with, the proposed use are equivalent to those of one or more in the used listed in the zone as allowable, and will not involve a greater level of activity, population density, intensity, traffic generation, parking, or noise than the uses listed in the zone;
 - II. The purposes of the applicable zone and conformance with the goals of the Specific Plan; and
 - III. The goals and polices of the General Plan.
 - IV. All such final determinations shall be recorded in writing to include a finding that the proposed use is substantially similar to uses permitted in the proposed zone and consistent with the intent of the applicable zone. All use determinations shall be filed and treated as precedent for future use determination requests, and become a part of the Specific Plan.

Permitted Uses		
Land Use Type	DC	Compton Walk Front
Entertainment and Recreation		
Health/Fitness Facility <15,000 sf	P	P
Health/Fitness Facility >15,000 sf	M	M
Indoor/Outdoor Entertainment	M	M
Indoor/Outdoor Recreation	M	M
Nightclub	C	C
Park, playground	P	X
Plaza	P	P
Studio: art, dance, martial arts, music, etc.	P	M
Civic		
Government	P	P
Library, museum, or art gallery	P	P
Office, general, and medical	P	P
Meeting facility, public or private	C	M
School, public or private	P	M
Food and Drink		
Micro-Brewery ¹	C	C
Pub/Bar/Tavern ²	C	C
Restaurant ² , café, coffee shop	P	P
Restaurant ² w/ Outside dining ³	P	P
Any food and drink or retail establishment operating before 7:00 am or after 10:00 pm	M	M
Retail		
Alcohol/Tobacco Outlet ²	C	C
General Retail < 100,000 sq. ft.	P	P
General Retail > 100,001 sq. ft.	M	C
Cannabis Activities	X	X
Services		
Business/Professional service	P	P
Childcare Facility	P	P
Family Childcare Home ⁴	X	X
Hotel/Motel	P	P
Medical or residential care facility <7	X	X
Medical or residential care facility >7	X	X
Personal services	P	P
Personal services-restricted	M	C
Transitional/Supportive Housing	P	X
Emergency Shelters	X	X
Residential		
Dwelling: Multi- Family (Including any type of permanent housing and supportive housing)	P	X
Dwelling: Single- Family Attached	P	X
Group Home (<7 Persons)	P	X
Group Home (>7 persons)	C	X
Home Occupation, no clients	P	X
Home Occupation, with clients	M	X
Live/work unit	P	X

Single Room Occupancy	P	X
Senior Housing	P	X
Industrial		
Artisan Manufacturing	X	X
Wireless Communication Facilities (Ground / Rooftop)	C	X
Materials (interior or exterior) Storage	X	X
Light Manufacturing	X	X
Heavy Manufacturing	X	X
Warehouse/Distribution	X	X
Automobile Related		
Parking Facility, Public or Private ^{5, 6}	M	X
Service Station	X	X
<ol style="list-style-type: none"> 1. Must be in tandem with a sit down restaurant. 2. Alcoholic beverage sales establishments are subject to Chapter 31-50 of the Municipal code. Restaurants (bona fide eating places whose predominant function is the service of a full range of food selections in a sit-down setting with table service and little, if any emphases on 'take out' food with onsite accessory sales of alcohol) may incidentally serve alcoholic beverages with meals without need of a Conditional Use permit. 3. Outdoor dining is subject to the standards described in Section 4.6.4. 4. Only permitted in Single Family Homes. See Zoning Code Section 30-11.3. 5. Parking structures shall comply with the standards in Section 5.15.6. 6. When not a part of a larger development, otherwise permitted. 		

Figure 3-4. Permitted Uses, Downtown Core (DC) Zone.

Development Standards – Downtown Core Zone (DC)

3.3.4 Height		
Minimum	3 floors	
Maximum	10 floors	
3.3.5 Setbacks		
Front Setbacks	0 ft. min., 10 ft. max.	
Street Side Setback	5 ft. min., 10 ft. max.	
Interior Side Setback	0 ft. min.	
Rear Setback	0 ft. min., 10 ft. min. adjacent to residential	
3.3.6 Interior Lot Line Height Transitions		
Adjacent to Residential Use	45-degree plane starting 25' high at property line	
Adjacent to Residential Use with Concession	60-degree plane starting 25' high at property line	
3.3.7 Minimum Residential Unit Size		
Single Room Occupancy	200 sf	
Studio	300 sf	
1-bedroom	400 sf	
2-bedroom	700 sf	
3-bedroom	900 sf	
3.3.8 Residential Open Space		
Common open space (for residents)	15% min. of site area	
Open space required per unit	150 sf and 30% attached to unit	
3.3.9 Minimum Parking Ratios		
	Unrestricted	Affordable
1+ bedroom units	1	0.5
SRO/studio units	0.5	None
Senior Housing (any size)	0.5	None
Guest Parking	0.15	0.15
Commercial Office	2/1,000 sf	
Retail or Restaurant	2.5/1,000 sf	
Other uses	50% less than CMC Table 30-21.A	
3.3.10 Frontage Types		
	Compton Walk	Other Streets
Arcade (Figure 3-19)	Permitted	Permitted
Gallery (Figure 3-20)	Permitted	Permitted
Terrace (Figure 3-21)	Not Permitted	Permitted
Shopfront (Figure 3-22)	Permitted	Permitted
Parking (Figure 3-23)	Not Permitted	Permitted
Lobby (Figure 3-24)	Permitted	Permitted
Stoop/Porch (Figure 3-25)	Not Permitted	Permitted
Front Yard (Figure 3-26)	Not Permitted	Not Permitted
3.3.11 Internal Streets and Publicly Accessible Open Space		
Publicly-accessible open space	8% of lot area	

Figure 3-5. Development Standards – Downtown Core (DC) Zone.

4. Height.

- A. Minimum height. Each new development project shall include at least one building of three habitable stories or higher.
- B. Maximum height. All new construction shall have a maximum height of ten stories.
- C. Architectural projections, towers and elevator shafts may exceed the last allowed story by 15 feet, as long as they are no longer than 30 feet long on any side.
- D. Mechanical equipment may exceed the vertical height limit, provided that it is not visible from an adjacent street, public right-of-way, or ground level private property.
- E. Roof decks, gardens and recreational amenities are permitted and encouraged on the top floor of developments.

5. Setbacks, Projections, and Encroachments.

- A. Front setback. No front setback is required. Buildings must also not be set back more than ten feet.
- B. Street side setback. Where the side of a lot abuts a public street, the building shall be set back no less than 5 feet and no more than 10 feet. Side frontages shall comply with frontage standards in Section 3.3.10.
- C. Interior side setbacks are not required.
- D. Rear setback. No rear setback is required, except when adjacent to residential uses, in which case the minimum setback is ten feet.
- E. Portions of buildings and structures including marquees, awnings, shade structures, signs, and architectural features may encroach into the public right-of-way subject to encroachment permitting requirements of the City Zoning Code, and/or the review and approval by the City Engineer, or designee. Encroachments or projections into the right-of-way shall not include habitable space.
- F. No projection shall be less than 9 feet above sidewalk grade, except for A-frame signage.
- G. Projections into applicable setback areas are permitted as described in Figure 3-6 (Projections into Setback Areas). No projection shall be over a roadway or alley (sidewalk exempted).

Projections into Setback Areas				
Projection	Front	Rear	Side	Street Side
Arcade	10	N	N	10
Architectural Features ¹	3	3	3	3
Balcony	7	6	6	6
Gallery	10	N	N	10
Shopfront (awning, bay window, or projecting shade structure)	10	N	N	10
Signage	P	N	N	P
Stoop	5	N	N	5
Terrace	6	N	N	6

Figure 3-6. Projections into Setback Areas.

- H. Awnings shall meet the following minimum standards:
 - I. Awnings shall be constructed of metal, canvas, or fire-resistant acrylic. Use of plastic and vinyl is prohibited.
 - II. Back-lit and waterfall awnings are prohibited.
 - III. Dome awnings are permitted only above building entryways. Only the address number or building name may be printed on the awning.

6. Interior Lot Line Height Transitions.

- A. Wherever an interior lot line abuts a single-family or multi-family residential building, new development shall not exceed the height established by an imaginary plane beginning 25 feet above the property line and rising up at a 45-degree angle toward the interior of the new development site.
- B. When a project utilizes a Public Benefit Concession to increase the transitional height pursuant to Section 3.12, wherever an interior lot line abuts a residential use, new development shall not exceed the height established by an imaginary plane beginning 25 feet above the property line and rising up at a 60-degree angle toward the interior of the new development site.

¹ Not to project into street or alley.



Required interior lot line height transition under base (left) and public benefit concession (right) standards.

7. Minimum Unit Size.

- A. The minimum residential unit size shall be 200 square feet for single-room occupancy units, 300 square feet for studio units, 400 square feet for one-bedroom units, 700 square feet for two-bedroom units and 900 square feet for three-bedroom units.

8. Residential Open Space.

- A. All projects containing a residential component shall include a common open space for residents, or series of common open spaces, no smaller than 15% of the lot area. This common open space may be located on top of roofs, podium parking garages, balconies, or commercial establishments if desired.
- B. A minimum of 150 square feet of open space shall be provided for each residential unit on-site. This requirement may be met through private or common open spaces. Up to 20% of the requirement may be met through publicly accessible on-site open space. A minimum of 30% of this requirement shall be met through private open space which is attached to the unit, such as balconies or private patios.

9. Minimum Parking Ratios.

- A. The minimum number of parking spaces required is given in Figure 3-5.
- B. Affordable residential unit parking ratios apply only to units which have a recorded affordability covenant to serve households making 80% of area median income or less for at least 30 years.

10. Frontage Types.

- A. Buildings shall incorporate one or more of the allowable frontage types on the first floor of each building where the property line meets a public street right-of-way or plaza. While the majority of each building frontage should meet this criteria, exceptions are permitted at driveways, spaces between buildings, etc.
- B. Standards for each frontage type are given in Figure 3-5. The standards for “Compton Walk” frontages shall apply to any frontage located along the Compton Walk as it is labeled in Figure 2-11.

11. Internal Streets and Publicly-Accessible Open Space.

- A. Developments shall construct internal streets and publicly accessible open space in accordance with the regulations established in Section 5.7. If the footprint of the proposed development is less than the full extent of one of the two shopping centers,

the applicant shall present a phasing plan demonstrating how the initial phase of the project fits within a larger plan to fulfill these regulations.

- B. Each large development site (Civic Center, Renaissance Center, Towne Center) shall dedicate at least eight percent (8%) of the lot area to publicly-accessible open space in the form of parks, plazas, pedestrian malls, or mid-block passthroughs. These spaces must be accessible to the general public between the hours of 7:00 am and 10:00 pm daily and not be accessible by private vehicle.

3.4 Downtown Transition (DT) Zone

1. Land Use Regulations.

Figure 3-7 (Permitted Uses) describes the land use regulations for the Downtown Transition Zone. The regulations for each zone are established as follows:

- A. "P" – Uses permitted by right that require no discretionary review if in compliance with all standards
- B. "M" – Uses subject to a Minor Use Permit following administrative review by the Community Development Department Director
- C. "C" – Uses subject to a Conditional Use Permit following discretionary review and public hearing by the Planning Commission
- D. "T" – Uses subject to a Temporary Use Permit following administrative review by the Community Development Department Director
- E. "Compton Walk Front" refers to uses which are permitted on the first floor frontage of the area marked on Figure 2-10 as the Compton Walk.

2. Establishment of Permitted Uses.

- A. Any of the land uses permitted in Figure 3-7 (Permitted Uses), may be established on any lot within the Downtown Transition zone, subject to the permit requirement listed in the Figure 3-7 (Permitted Uses), and in compliance with all applicable standards of this Chapter.
- B. If a proposed use is not listed in Figure 3-7 (Permitted Uses), the use is not permitted unless the Community Development Department Director determines in writing that the proposed use is substantially similar to a use specified as a permitted or conditional use in that zone in compliance with Subsection 3 (Use Determination).

3. Use Determination.

- A. The Community Development Department Director shall have the authority to make an administrative determination as to whether a specific use is substantially similar to one of the listed used in Figure 3-7 (Permitted Uses). The Community Development Department Director may at his or her discretion refer the determination to the Planning Commission. In making a determination that a proposed use is or is not similar to those listed, the Community Development Department Director or Planning Commission shall consider:
 - I. Whether the characteristics of, and activities associated with, the proposed use are equivalent to those of one or more in the used listed in the zone as allowable, and will not involve a greater level of activity, population density, intensity, traffic generation, parking, or noise than the uses listed in the zone;
 - II. The purposes of the applicable zone and conformance with the goals of the Specific Plan; and
 - III. The goals and polices of the General Plan.
 - IV. All such final determinations shall be recorded in writing to include a finding that the proposed use is substantially similar to uses permitted in the proposed zone and consistent with the intent of the applicable zone. All use determinations shall be filed and treated as precedent for future use determination requests.

Permitted Uses		
Land Use Type	DT	Compton Walk Front
Entertainment and Recreation		
Health/Fitness Facility <15,000 sf	P	P
Health/Fitness Facility >15,000 sf	M	M
Indoor/Outdoor Entertainment	M	M
Indoor/Outdoor Recreation	M	M
Nightclub	C	C
Park, playground	P	X
Plaza	P	P
Studio: art, dance, martial arts, music, etc.	P	M
Civic		
Government	P	P
Library, museum, or art gallery	P	P
Office, general	P	P
Meeting facility, public or private	M	M
School, public or private	P	M
Food and Drink		
Micro-Brewery ¹	C	C
Pub/Bar/Tavern ²	C	C
Restaurant, café, coffee shop ²	P	P
Restaurant w/ Outside dining ³	P	P
Any food and drink or retail establishment operating before 7:00 am or after 10:00 pm	M	M
Retail		
Alcohol/Tobacco Outlet ²	C	C
General Retail < 100,000 sq. ft.	P	P
General Retail > 100,001 sq. ft.	C	C
Cannabis Activities	X	X
Services		
Business/Professional service	P	P
Childcare Facility	P	P
Family Childcare Home ⁴	X	X
Hotel/Motel	P	P
Medical or residential care facility <7	X	X
Medical or residential care facility >7	X	X
Personal services	P	P
Personal services-restricted	M	C
Transitional/Supportive Housing	P	X
Emergency Shelter	C	C
Residential		
Dwelling: Multi- Family (Including any type of permanent housing and supportive housing)	P	X
Dwelling: Single- Family Detached	X	X
Dwelling: Single- Family Attached	P	X
Group Home (<7 Persons)	P	X
Group Home (>7 persons)	C	X
Home Occupation, no clients	P	X
Home Occupation, with clients	M	X

Live/work unit	P	X
Single Room Occupancy	P	X
Senior Housing	P	X
Industrial		
Artisan Manufacturing	X	X
Wireless Communication Facilities (Ground / Rooftop)	C	X
Materials Storage	X	X
Light Manufacturing	X	X
Heavy Manufacturing	X	X
Warehouse/Distribution	X	X
Automobile Related		
Parking Facility, Public or Private ^{5, 6}	M	X
Service Station	X	X
<ol style="list-style-type: none"> 1. Must be in tandem with a sit down restaurant. 2. Alcoholic beverage sales establishments are subject to Chapter 31-50 of the Municipal code. Restaurants (bona fide eating places whose predominant function is the service of a full range of food selections in a sit-down setting with table service and little, if any emphasizes on 'take out' food with onsite accessory sales of alcohol) may incidentally serve alcoholic beverages with meals without need of a Conditional Use permit. 3. Outdoor dining is subject to the standards described in Section 4.6.4. 4. Only permitted in Single Family Homes. See Zoning Code Section 30-11.3. 5. Parking structures shall comply with the standards in Subsection 5.15.6. 6. When not a part of a larger development, otherwise permitted. 		

Figure 3-7. Permitted Uses, Downtown Transition (DT) Zone.

Development Standards – Downtown Transition Zone (DT)

3.4.4 Height		
Minimum	3 floors	
Maximum	6 floors	
3.4.5 Setbacks		
Front Setbacks	0 ft. min., 10 ft. max.	
Street Side Setback	5 ft. min., 10 ft. max.	
Interior Side Setback	0 ft. min.	
Rear Setback	0 ft. min., 10 ft. min. adjacent to residential	
3.4.6 Interior Lot Line Height Transitions		
Adjacent to Residential Use	45-degree plane starting 25' high at property line	
Adjacent to Residential Use with Concession	60-degree plane starting 25' high at property line	
3.4.7 Minimum Residential Unit Size		
Single Room Occupancy	200 sf	
Studio	300 sf	
1-bedroom	400 sf	
2-bedroom	700 sf	
3-bedroom	900 sf	
3.4.8 Residential Open Space		
Common open space (for residents)	15% min. of site area	
Open space required per unit	150 sf and 30% attached to unit	
3.4.9 Minimum Parking Ratios		
	Unrestricted	Affordable
1+ bedroom units	1	0.5
SRO/studio units	0.5	None
Senior Housing (any size)	0.5	None
Guest Parking (rounded up to next whole number)	0.15	0.15
Commercial Office	2/1,000 sf	
Retail or Restaurant	2.5/1,000 sf	
Other uses	50% less than CMC Table 30-21.A	
3.4.10 Frontage Types		
	Compton Walk	Other Streets
Arcade (Figure 3-19)	Permitted	Permitted
Gallery (Figure 3-20)	Permitted	Permitted
Terrace (Figure 3-21)	Not Permitted	Permitted
Shopfront (Figure 3-22)	Permitted	Permitted
Parking (Figure 3-23)	Not Permitted	Permitted
Lobby (Figure 3-24)	Permitted	Permitted
Stoop/Porch (Figure 3-25)	Not Permitted	Permitted
Yard (Figure 3-26)	Not Permitted	Not Permitted

Figure 3-8. Development Standards – Downtown Transition Zone (DT).

4. Height.

- A. Minimum height. Each new development project shall include at least one building of three stories or higher.
- B. Maximum height. All new construction shall have a maximum height of six stories.
- C. Architectural projections, towers and elevator shafts may exceed the vertical height limit to the zone by 15 feet, as long as they are no longer than 30 feet long on any side.
- D. Mechanical equipment may exceed the vertical height limit, provided that it is not visible from an adjacent street, public right-of-way, or ground level private property.
- E. Roof decks, gardens and recreational amenities are strongly encouraged on the top floor of developments.
- F. Projects located on the north side of Compton Boulevard shall conduct a shadow study to determine if any shadow-sensitive land uses, defined as routinely usable outdoor spaces associated with residential, recreational or institutional uses, are anticipated to be shaded for more than three hours between 10:00 am and 4:00 pm Pacific Daylight Time on the winter solstice. Should any shadow-sensitive land uses meet this criteria, the applicant shall conduct a special community meeting noticing the property owners and residents of the parcel containing said shadow-sensitive land uses, and shall demonstrate good faith efforts to provide benefits to these residents within the development.

5. Setbacks, Projections, and Encroachments.

- A. Front setbacks. No front setback is required. Buildings must also not be set back more than ten feet.
- B. Street side setback. Where the side of a lot abuts a public street, the building shall be set back no less than 5 feet and no more than 10 feet. Side frontages shall comply with frontage standards in Section 3.3.10.
- C. Interior side setbacks are not required.
- D. Rear setback. No rear setback is required, except when adjacent to residential uses, in which case the minimum setback is ten feet.
- E. Portions of buildings and structures including marquees, awnings, shade structures, signs, and architectural features may encroach into the public right-of-way subject to encroachment permitting requirements of the City Zoning Code, and/or the review and approval by the City Engineer, or designee. Encroachments or projections into the right-of-way shall not include habitable space.
- F. No projection shall be less than 9 feet above sidewalk grade, except for A-frame signage.
- G. Projections into applicable setback areas are permitted as described in Figure 3-9 (Projections into Setback Areas). No projection shall be over a roadway or alley (sidewalk exempted).

Projections into Setback Areas				
Projection	Front	Rear	Side	Street Side
Arcade	10	N	N	10
Architectural Features	3	3	3	3
Balcony	7	6	6	6
Gallery	10	N	N	10
Shopfront (awning, bay window, or projecting shade structure)	10	N	N	10
Signage	P	N	N	P
Stoop	5	N	N	5
Terrace	6	N	N	6

Figure 3-9. Projections into Setback Areas.

- H. Awnings shall meet the following minimum standards:
 - I. Awnings shall be constructed of metal, canvas, or fire-resistant acrylic. Use of plastic and vinyl is prohibited.
 - II. Back-lit and waterfall awnings are prohibited.
 - III. Dome awnings are permitted only above building entryways. Only the address number or building name may be printed on the awning.

6. Interior Lot Line Height Transitions.

- A. Wherever an interior lot line abuts a single-family or multi-family residential building built before original adoption of this Specific Plan, new development shall not exceed the height established by an imaginary plane beginning 25 feet above the property line and rising up at a 45-degree angle toward the interior of the new development site.
- B. When a project utilizes a Public Benefit Concession to increase the transitional height pursuant to Section 3.12, wherever an interior lot line abuts a residential use built before original adoption of this Specific Plan, new development shall not exceed the height established by an imaginary plane beginning 25 feet above the property line and rising up at a 60-degree angle toward the interior of the new development site.



Required interior lot line height transition under base (left) and public benefit concession (right) standards.

7. Minimum Unit Size.

- A. The minimum residential unit size shall be 200 square feet for single-room occupancy units, 300 square feet for studio units, 400 square feet for one-bedroom units, 700 square feet for two-bedroom units and 900 square feet for three-bedroom units.

8. Residential Open Space.

- A. All projects containing a residential component shall include a common open space, for residents, or series of common open spaces, no smaller than 15% of the lot area. This common open space may be located on top of roofs, podium parking garages, balconies, or commercial establishments if desired.
- B. A minimum of 150 square feet of open space shall be provided for each residential unit on-site. This requirement may be met through private or common open spaces. Up to 20% of the requirement may be met through publicly accessible on-site open space. A minimum of 30% of this requirement shall be met through private open space which is attached to the unit, such as balconies or private patios.

9. Minimum Parking Ratios.

- A. The minimum number of parking spaces required is given in Figure 3-8.
- B. Affordable residential unit parking ratios apply only to units which have a recorded affordability covenant to serve households making 80% of area median income or less for at least 30 years.

10. Frontage Types.

- A. Buildings shall incorporate one or more of the allowable frontage types on the first floor of each building where the property line meets a public street right-of-way or plaza. While the majority of each building frontage should meet this criteria, exceptions are permitted at driveways, spaces between buildings, etc.
- B. Standards for each frontage type are given in Figure 3-8. The standards for “Compton Walk” frontages shall apply to any frontage located along the Compton Walk as it is labeled in Figure 2-11.

3.5 Residential Urban (RU) Zone

1. Land Use Regulations.

Figure 3-10 (Permitted Uses) describes the land use regulations for the Residential Urban Zone. The regulations for each zone are established as follows:

- A. "P" – Uses permitted by right that require no discretionary review if in compliance with all standards
- B. "M" – Uses subject to a Minor Use Permit following administrative review by the Community Development Department Director
- C. "C" – Uses subject to a Conditional Use Permit following discretionary review and public hearing by the Planning Commission
- D. "T" – Uses subject to a Temporary Use Permit following administrative review by the Community Development Department Director

2. Establishment of Permitted Uses.

- A. Any of the land uses permitted in Figure 3-10 (Permitted Uses), may be established on any lot within the Residential Urban zone, subject to the permit requirement listed in the Figure 3-10 (Permitted Uses), and in compliance with all applicable standards of this Chapter.
- B. If a proposed use is not listed in Figure 3-10 (Permitted Uses), the use is not permitted unless the Community Development Department Director determines in writing that the proposed use is substantially similar to a use specified as a permitted or conditional use in that zone in compliance with Subsection 3 (Use Determination).

3. Use Determination.

- A. The Community Development Department Director shall have the authority to make an administrative determination as to whether a specific use is substantially similar to one of the listed used in Figure 3-10 (Permitted Uses). The Community Development Department Director may at his or her discretion refer the determination to the Planning Commission. In making a determination that a proposed use is or is not similar to those listed, the Community Development Department Director or Planning Commission shall consider:
 - I. Whether the characteristics of, and activities associated with, the proposed use are equivalent to those of one or more in the used listed in the zone as allowable, and will not involve a greater level of activity, population density, intensity, traffic generation, parking, or noise than the uses listed in the zone;
 - II. The purposes of the applicable zone and conformance with the goals of the Specific Plan; and
 - III. The goals and polices of the General Plan.
 - IV. All such final determinations shall be recorded in writing to include a finding that the proposed use is substantially similar to uses permitted in the proposed zone and consistent with the intent of the applicable zone. All use determinations shall be filed and treated as precedent for future use determination requests.

Permitted Uses	
Land Use Type	RU
Entertainment and Recreation	
Health/Fitness Facility <15,000 sf	X
Health/Fitness Facility >15,000 sf	X
Indoor/Outdoor Entertainment	X
Indoor/Outdoor Recreation	X
Nightclub	X
Park, playground	M
Plaza	M
Studio: art, dance, martial arts, music, etc.	X
Civic	
Government	P
Library, museum, or art gallery	X
Office, general	X
Meeting facility, public or private	X
School, public or private	P
Food and Drink	
Micro-Brewery ¹	X
Pub/Bar/Tavern ²	X
Restaurant, café, coffee shop ²	X
Restaurant w/ Outside dining ³	X
Any food and drink or retail establishment operating before 7:00 am or after 10:00 pm	X
Retail	
Alcohol/Tobacco Outlet ²	X
Small retail < 2,500 sq. ft.	P
General Retail > 2,500 sq. ft. and < 100,000 sq. ft.	X
General Retail > 100,001 sq. ft.	X
Cannabis Activities	X
Services	
Business/Professional service	X
Childcare Facility	P
Family Childcare Home ⁴	X
Hotel/Motel	X
Medical or residential care facility <7	P
Medical or residential care facility >7	C
Personal services	X
Personal services-restricted	X
Transitional/Supportive Housing	P
Emergency Shelter	X
Residential	
Dwelling: Multi- Family (Including supportive housing)	P
Dwelling: Single- Family Detached	P
Dwelling: Single- Family Attached	P
Group Home (<7 Persons)	P
Group Home (>7 persons)	C
Home Occupation, no clients	P
Home Occupation, with clients	P

Live/work unit	P
Single Room Occupancy	P
Senior Housing	P
Industrial	
Artisan Manufacturing	X
Wireless Communication Facilities (Ground / Rooftop)	X
Materials Storage	X
Light Manufacturing	X
Heavy Manufacturing	X
Warehouse/Distribution	X
Automobile Related	
Parking Facility, Public or Private ^{5, 6}	P
Service Station	X
<ol style="list-style-type: none"> 1. Must be in tandem with a sit down restaurant. 2. Alcoholic beverage sales establishments are subject to Chapter 31-50 of the Municipal code. Restaurants (bona fide eating places whose predominant function is the service of a full range of food selections in a sit-down setting with table service and little, if any emphasizes on 'take out' food with onsite accessory sales of alcohol) may incidentally serve alcoholic beverages with meals without need of a Conditional Use permit. 3. Outdoor dining is subject to the standards described in Section 4.6.4. 4. Only permitted in Single Family Homes. See Zoning Code Section 30-11.3. 5. Parking structures shall comply with the standards in Subsection 5.15.6. 6. When not a part of a larger development, otherwise permitted. 	

Figure 3-10. Permitted Uses, Residential Urban (RU) Zone.

Development Standards – Residential Urban (RU)

3.5.4 Height

Minimum	None
Maximum	3 floors (5 floors fronting on Willowbrook)

3.5.5 Setbacks

Front Setbacks	10 ft. min., 20 ft. max.
Side Setback	5 ft. min.
Rear Setback	10 ft. min.

3.5.6 Interior Lot Line Height Transitions

Adjacent to Residential Use	45-degree plane starting 15' high at property line.
Adjacent to Residential Use with Concession	60-degree plane starting at 15' high at property line.

3.5.7 Minimum Residential Unit Size

Single Room Occupancy	200 sf
Studio	300 sf
1-bedroom	550 sf
2-bedroom	750 sf
3-bedroom	950 sf

3.5.8 Residential Open Space

Common open space	Not required
Open space required per unit	200 sf

3.5.9 Minimum Parking Ratios

	Unrestricted	Affordable
1+ bedroom units	1	0.5
SRO/studio units	1	0.5
Senior Housing (any size)	0.5	0.5
Guest Parking	0.1	0.1
Other uses	30% less than CMC Table 30-21.A	

3.5.10 Frontage Types

Arcade	Not Permitted
Gallery	Not Permitted
Terrace	Permitted
Shopfront	Permitted
Parking	Not Permitted
Lobby	Permitted
Stoop/Porch	Permitted
Front Yard	Permitted

Figure 3-11. Development Standards – Residential Urban (RU).

4. Height.

- A. Minimum height. There is no minimum height for new construction.
- B. Maximum height. All new construction shall have a maximum height of three stories. However, where the property fronts Alameda Street, Willowbrook Avenue East, or Willowbrook Avenue West, the maximum height is five stories.
- C. Architectural projections, towers and elevator shafts may exceed the vertical height limit to the zone by 15 feet, as long as they are no longer than 30 feet long on any side.
- D. Mechanical equipment may exceed the vertical height limit, provided that it is not visible from an adjacent street, public right-of-way, or ground level private property.
- E. Roof decks, gardens and recreational amenities are permitted on the top floor of developments.

5. Setbacks, Projections, and Encroachments.

- A. Setbacks. The minimum front setback is ten feet and the maximum front setback is twenty feet.
- B. Side Setback. The minimum side setback is five feet.
- C. Rear Setback. The minimum rear setback is ten feet.
- D. Portions of buildings and structures including marquees, awnings, shade structures, signs, and architectural features may encroach into the public right-of-way subject to encroachment permitting requirements of the City Zoning Code, and/or the review and approval by the City Engineer, or designee. Encroachments or projections into the right-of-way shall not include habitable space.
- E. No projection shall be less than 9 feet above sidewalk grade, except for A-frame signage.
- F. Projections into applicable setback areas are permitted as described in Figure 3-12 (Projections into Setback Areas). No projection shall be over a roadway or alley (sidewalk exempted).

Projections into Setback Areas				
Projection	Front	Rear	Side	Street Side
Arcade	10	N	N	10
Architectural Features	3	3	3	3
Balcony	7	6	6	6
Gallery	10	N	N	10
Shopfront (awning, bay window, or projecting shade structure)	10	N	N	10
Signage	P	N	N	P
Stoop	5	N	N	5
Terrace	6	N	N	6

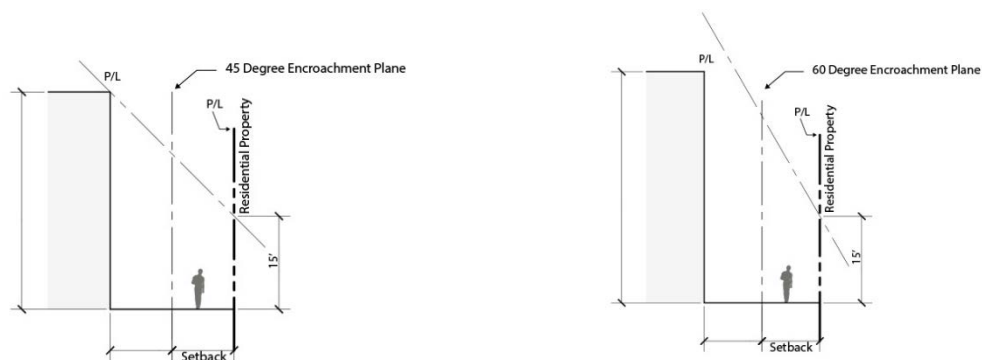
Figure 3-12. Projections into Setback Areas.

- G. Awnings shall meet the following minimum standards:
 - I. Awnings shall be constructed of metal, canvas, or fire-resistant acrylic. Use of plastic and vinyl is prohibited.
 - II. Back-lit and waterfall awnings are prohibited.

- III. Dome awnings are permitted only above building entryways. Only the address number or building name may be printed on the awning.

6. Interior Lot Line Height Transitions.

- A. Wherever an interior lot line abuts a single-family or multi-family residential building built before original adoption of this Specific Plan, new development shall not exceed the height established by an imaginary plane beginning 15 feet above the property line and rising up at a 45-degree angle toward the interior of the new development site.
- B. When a project utilizes a Public Benefit Concession to increase the transitional height pursuant to Section 3.12, wherever an interior lot line abuts a residential use built before original adoption of this Specific Plan, new development shall not exceed the height established by an imaginary plane beginning 15 feet above the property line and rising up at a 60-degree angle toward the interior of the new development site.



Required interior lot line height transition under base (left) and public benefit concession (right) standards.

7. Minimum Unit Size.

- A. The minimum residential unit size shall be 200 square feet for single-room occupancy units, 300 square feet for studio units, 550 square feet for one-bedroom units, 750 square feet for two-bedroom units and 950 square feet for three-bedroom units.

8. Residential Open Space.

- A. A minimum of 200 square feet of open space shall be provided for each residential unit on-site. This requirement may be met through private or common open spaces, however at least 50 sq. ft. minimum shall be in an attached Patio or Balcony.

9. Minimum Parking Ratios.

- A. The minimum number of parking spaces required is given in Figure 3-11.
- B. Affordable residential unit parking ratios apply only to units which have a recorded affordability covenant to serve households making 80% of area median income or less for at least 30 years.
- C. Local commercial uses (i.e. corner stores) of 1,500 square feet or less are exempt from parking requirements.

10. Frontage Types.

- A. Buildings shall incorporate one or more of the allowable frontage types on the first floor of each building where the property line meets a public street right-of-way or plaza.

While the majority of each building frontage should meet this criteria, exceptions are permitted at driveways, spaces between buildings, etc.

- B. The shopfront frontage type is permitted on corners only, except on Alameda Street East.
- C. Standards for each frontage type are given in Figure 3-11.

3.6 Neighborhood Corridor (NC) Zone

1. Land Use Regulations.

Figure 3-13 (Permitted Uses) describes the land use regulations for the Neighborhood Corridor Zone. The regulations for each zone are established as follows:

- A. "P" – Uses permitted by right that require no discretionary review if in compliance with all standards
- B. "M" – Uses subject to a Minor Use Permit following administrative review by the Community Development Department Director
- C. "C" – Uses subject to a Conditional Use Permit following discretionary review and public hearing by the Planning Commission
- D. "T" – Uses subject to a Temporary Use Permit following administrative review by the Community Development Department Director

2. Establishment of Permitted Uses.

- A. Any of the land uses permitted in Figure 3-13 (Permitted Uses), may be established on any lot within the Neighborhood Corridor zone, subject to the permit requirement listed in the Figure 3-13 (Permitted Uses), and in compliance with all applicable standards of this Chapter.
- B. If a proposed use is not listed in Figure 3-13 (Permitted Uses), the use is not permitted unless the Community Development Department Director determines in writing that the proposed use is substantially similar to a use specified as a permitted or conditional use in that zone in compliance with Subsection 3 (Use Determination).

3. Use Determination.

- A. The Community Development Department Director shall have the authority to make an administrative determination as to whether a specific use is substantially similar to one of the listed used in Figure 3-13 (Permitted Uses). The Community Development Department Director may at his or her discretion refer the determination to the Planning Commission. In making a determination that a proposed use is or is not similar to those listed, the Community Development Department Director or Planning Commission shall consider:
 - I. Whether the characteristics of, and activities associated with, the proposed use are equivalent to those of one or more in the used listed in the zone as allowable, and will not involve a greater level of activity, population density, intensity, traffic generation, parking, or noise than the uses listed in the zone;
 - II. The purposes of the applicable zone and conformance with the goals of the Specific Plan; and
 - III. The goals and polices of the General Plan.
 - IV. All such final determinations shall be recorded in writing to include a finding that the proposed use is substantially similar to uses permitted in the proposed zone and consistent with the intent of the applicable zone. All use determinations shall be filed and treated as precedent for future use determination requests.

Permitted Uses	
Land Use Type	NC
Entertainment and Recreation	
Health/Fitness Facility <15,000 sf	P
Health/Fitness Facility >15,000 sf	P
Indoor/Outdoor Entertainment	M
Indoor/Outdoor Recreation	M
Nightclub	X
Park, playground	C
Plaza	P
Studio: art, dance, martial arts, music, etc.	P
Civic	
Government	P
Library, museum, or art gallery	C
Office, general	P
Meeting facility, public or private	M
School, public or private	P
Food and Drink	
Micro-Brewery ¹	X
Pub/Bar/Tavern ²	C
Restaurant, café, coffee shop ²	P
Restaurant w/ Outside dining ³	P
Any food and drink or retail establishment operating before 7:00 am or after 10:00 pm	C
Retail	
Liquor Store/Tobacco Store	C
General Retail < 100,000 sq. ft.	P
General Retail > 100,001 sq. ft.	P
Cannabis Activities	X
Services	
Business/Professional service	P
Childcare Facility	P
Family Childcare Home ⁴	X
Hotel/Motel	P
Medical or residential care facility <7	P
Medical or residential care facility >7	M
Personal services	P
Personal services-restricted	X
Transitional and Supportive Housing	P
Residential	
Dwelling: Multi- Family (Including any type of permanent housing and supportive housing)	P
Dwelling: Single- Family Detached	P
Dwelling: Single- Family Attached	P
Group Home (<7 Persons)	P
Group Home (>7 persons)	C
Home Occupation, no clients	X
Home Occupation, with clients	X
Live/work unit	P

Single Room Occupancy	P
Senior Housing	P
Industrial	
Artisan Manufacturing	X
Wireless Communication Facilities (Ground / Rooftop)	C
Materials Storage	X
Light Manufacturing	X
Heavy Manufacturing	X
Warehouse/Distribution	X
Automobile Related	
Parking Facility, Public or Private ^{5, 6}	P
Service Station	X
<ol style="list-style-type: none"> 1. Must be in tandem with a sit down restaurant. 2. Alcoholic beverage sales establishments are subject to Chapter 31-50 of the Municipal code. Restaurants (bona fide eating places whose predominant function is the service of a full range of food selections in a sit-down setting with table service and little, if any emphasizes on 'take out' food with onsite accessory sales of alcohol) may incidentally serve alcoholic beverages with meals without need of a Conditional Use permit. 3. Outdoor dining is subject to the standards described in Section 4.6.4. 4. Only permitted in Single Family Homes. See Zoning Code Section 30-11.3. 5. Parking structures shall comply with the standards in Section 5.15.6. 6. When not a part of a larger development, otherwise permitted. 	

Figure 3-13. Permitted Uses, Neighborhood Corridor (NC) Zone.

Development Standards – Neighborhood Corridor (NC)

3.6.4 Height

Minimum	NP
Maximum	5 floors

3.6.5 Setbacks

Front Setbacks	0 ft. min., 15 ft. max.
Street Side Setback	5 ft. min., 10 ft. max.
Interior Side Setback	0 ft. min., 10 ft. min. adjacent to residential.
Rear Setback	10 ft. min.

3.6.6 Interior Lot Line Height Transitions

Adjacent to Residential Use	45-degree plane starting 15' high at property line
Adjacent to Residential Use with Concession	60-degree plane starting at 15' high at property line

3.6.7 Minimum Residential Unit Size

Single Room Occupancy	200 sf
Studio	300 sf
1-bedroom	500 sf
2-bedroom	700 sf
3-bedroom	900 sf

3.6.8 Residential Open Space

Common open space (for residents)	15% minimum of site area.
Open space required per unit	150 sf and 30% attached to unit.

3.6.9 Minimum Parking Ratios

	Unrestricted	Affordable
1+ bedroom units	1	0.5
SRO/studio units	1	0.5
Senior Housing (any size)	0.5	0.5
Guest Parking	0.15	0.15
Commercial Office	3/1,000 sf	
Retail or Restaurant	3.5/1,000 sf	
Other uses	30% less than CMC Table 30-21.A	

3.6.10 Frontage Types

	Commercial Street	Residential Street
Arcade (Figure 3-19)	Permitted	Not Permitted
Gallery (Figure 3-20)	Permitted	Not Permitted
Terrace (Figure 3-21)	Permitted	Not Permitted
Shopfront (Figure 3-22)	Permitted	Not Permitted
Parking (Figure 3-23)	Permitted	Permitted
Lobby (Figure 3-24)	Permitted	Not Permitted
Stoop/Porch (Figure 3-25)	Permitted	Permitted
Front Yard (Figure 3-26)	Not Permitted	Permitted

Figure 3-14. Permitted Uses.

4. Height.

- A. Minimum height. There is no minimum height for new construction.
- B. Maximum height. All new construction shall have a maximum height of five stories.
- C. Architectural projections, towers and elevator shafts may exceed the vertical height limit to the zone by 15 feet, as long as they are no longer than 20 feet long on any side.
- D. Mechanical equipment may exceed the vertical height limit, provided that it is not visible from an adjacent street, public right-of-way, or ground level private property.
- E. Roof decks, gardens, and recreational amenities are strongly encouraged on the top floor of developments.
- F. Projects located on the north side of Rosecrans Avenue or Palmer Street shall conduct a shadow study to determine if any shadow-sensitive land uses, defined as routinely usable outdoor spaces associated with residential, recreational or institutional uses, are anticipated to be shaded for more than three hours between 10:00 am and 4:00 pm Pacific Daylight Time on the winter solstice. Should any shadow-sensitive land uses meet this criteria, the applicant shall conduct a special community meeting noticing the property owners and residents of the parcel containing said shadow-sensitive land uses, and shall demonstrate good faith efforts to provide benefits to these residents within the development.

5. Setbacks, Projections, and Encroachments.

- A. Front Setbacks. No front setback is required. Buildings must also not be set back more than fifteen feet.
- B. Street side setback. Where the side of a lot abuts a public street, the building shall be set back no less than 5 feet and no more than 10 feet. Side frontages shall comply with frontage standards in Section 3.3.10.
- C. Interior Side setback. No interior side setback is required. Buildings must also not be set back less than ten feet, if the building is adjacent to an existing residential use.
- D. Rear setback. The minimum rear setback is ten feet.
- E. Portions of buildings and structures including marquees, awnings, shade structures, signs, and architectural features may encroach into the public right-of-way subject to encroachment permitting requirements of the City Zoning Code, and/or the review and approval by the City Engineer, or designee. Encroachments or projections into the right-of-way shall not include habitable space.
- F. No projection shall be less than 9 feet above sidewalk grade, except for A-frame signage.
- G. Projections into applicable setback areas are permitted as described in Figure 3-15 (Projections into Setback Areas). No projection shall be over a roadway or alley (sidewalk exempted).

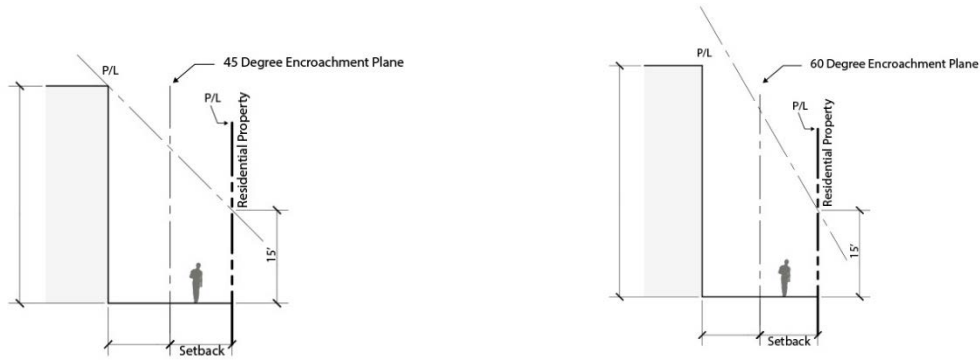
Projections into Setback Areas				
Projection	Front	Rear	Side	Street Side
Arcade	10	N	N	10
Architectural Features	3	3	3	3
Balcony	7	6	6	6
Gallery	10	N	N	10
Shopfront (awning, bay window, or projecting shade structure)	10	N	N	10
Signage	P	N	N	P
Stoop	5	N	N	5
Terrace	6	N	N	6

Figure 3-15. Projections into Setback Areas.

- H. Awnings shall meet the following minimum standards:
 - I. Awnings shall be constructed of metal, canvas, or fire-resistant acrylic. Use of plastic and vinyl is prohibited.
 - II. Back-lit and waterfall awnings are prohibited.
 - III. Dome awnings are permitted only above building entryways. Only the address number or building name may be printed on the awning.

6. Interior Lot Line Height Transitions.

- A. Wherever an interior lot line abuts a single-family or multi-family residential building built before original adoption of this Specific Plan, new development shall not exceed the height established by an imaginary plane beginning 15 feet above the property line and rising up at a 45-degree angle toward the interior of the new development site.
- B. When a project utilizes a Public Benefit Concession to increase the transitional height pursuant to Section 3.12, wherever an interior lot line abuts a residential use built before original adoption of this Specific Plan, new development shall not exceed the height established by an imaginary plane beginning 15 feet above the property line and rising up at a 60-degree angle toward the interior of the new development site.



Required interior lot line height transition under base (left) and public benefit concession (right) standards.

7. Minimum Unit Size.

- A. The minimum residential unit size shall be 200 square feet for single-room occupancy units, 300 square feet for studio units, 500 square feet for one-bedroom units, 700 square feet for two-bedroom units and 900 square feet for three-bedroom units.

8. Residential Open Space.

- A. All projects containing a residential component shall include a common open space for residents, or series of common open spaces, no smaller than 15% of the lot area. This common open space may be located on top of roofs, podium parking garages, balconies, or commercial establishments if desired.
- B. A minimum of 150 square feet of open space shall be provided for each residential unit on-site. This requirement may be met through private or common open spaces. Up to 20% of the requirement may be met through publicly accessible on-site open space. A minimum of 30% of this requirement shall be met through private open space which is attached to the unit, such as balconies or private patios.

9. Minimum Parking Ratios.

- A. The minimum number of parking spaces required is given in Figure 3-14.
- B. Affordable residential unit parking ratios apply only to units which have a recorded affordability covenant to serve households making 80% of area median income or less for at least 30 years.

10. Frontage Types.

- A. Buildings shall incorporate one or more of the allowable frontage types on the first floor of each building where the property line meets a public street right-of-way or plaza. While the majority of each building frontage should meet this criteria, exceptions are permitted at driveways, spaces between buildings, etc.
- B. Standards for each frontage type are given in Figure 3-14.

3.7 Urban Flexible (UF) Zone

1. Land Use Regulations.

Figure 3-16 (Permitted Uses) describes the land use regulations for the Urban Flexible Zone. The regulations for each zone are established as follows:

- A. "P" – Uses permitted by right that require no discretionary review if in compliance with all standards
- B. "M" – Uses subject to a Minor Use Permit following administrative review by the Community Development Department Director
- C. "C" – Uses subject to a Conditional Use Permit following discretionary review and public hearing by the Planning Commission
- D. "T" – Uses subject to a Temporary Use Permit following administrative review by the Community Development Department Director

2. Establishment of Permitted Uses.

- A. Any of the land uses permitted in Figure 3-16 (Permitted Uses), may be established on any lot within the Specific Plan, subject to the permit requirement listed in the Figure 3-16 (Permitted Uses), and in compliance with all applicable standards of this Chapter.
- B. If a proposed use is not listed in Figure 3-16 (Permitted Uses), the use is not permitted unless the Community Development Department Director determines in writing that the proposed use is substantially similar to a use specified as a permitted or conditional use in that zone in compliance with Subsection 3 (Use Determination).

3. Use Determination.

- A. The Community Development Department Director shall have the authority to make an administrative determination as to whether a specific use is substantially similar to one of the listed used in Figure 3-16 (Permitted Uses). The Community Development Department Director may at his or her discretion refer the determination to the Planning Commission. In making a determination that a proposed use is or is not similar to those listed, the Community Development Department Director or Planning Commission shall consider:
 - I. Whether the characteristics of, and activities associated with, the proposed use are equivalent to those of one or more in the used listed in the zone as allowable, and will not involve a greater level of activity, population density, intensity, traffic generation, parking, or noise than the uses listed in the zone;
 - II. The purposes of the applicable zone and conformance with the goals of the Specific Plan; and
 - III. The goals and polices of the General Plan.
 - IV. All such final determinations shall be recorded in writing to include a finding that the proposed use is substantially similar to uses permitted in the proposed zone and consistent with the intent of the applicable zone. All use determinations shall be filed and treated as precedent for future use determination requests.

Permitted Uses	
Land Use Type	UF
Entertainment and Recreation	
Health/Fitness Facility <15,000 sf	P
Health/Fitness Facility >15,000 sf	P
Indoor/Outdoor Entertainment	M
Indoor/Outdoor Recreation	P
Nightclub	C
Park, playground	P
Plaza	P
Studio: art, dance, martial arts, music, etc.	P
Civic	
Government	P
Library, museum, or art gallery	P
Office, general	P
Meeting facility, public or private	P
School, public or private	P
Food and Drink	
Micro-Brewery ¹	P
Pub/Bar/Tavern ²	C
Restaurant, café, coffee shop ²	P
Restaurant w/ Outside dining ³	P
Any food and drink or retail establishment operating before 7:00 am or after 10:00 pm	C
Retail	
Alcohol/Tobacco Outlet	C
General Retail < 100,000 sq. ft.	P
General Retail > 100,001 sq. ft.	P
Cannabis Activities	X
Services	
Business/Professional service	P
Childcare Facility	P
Family Childcare Home ⁴	X
Hotel/Motel	P
Medical or residential care facility <7	X
Medical or residential care facility >7	X
Personal services	X
Personal services-restricted	X
Supportive/Transitional Housing	X
Emergency Shelter	C
Residential	
Dwelling: Multi- Family (Including any type of permanent housing and supportive housing)	P
Dwelling: Single- Family Detached	P
Dwelling: Single- Family Attached	P
Group Home (<7 Persons)	C
Group Home (>7 persons)	C
Home Occupation, no clients	P
Home Occupation, with clients	P

Live/work unit	P
Single Room Occupancy	P
Senior Housing	P
Industrial	
Artisan Manufacturing ⁷	P
Wireless Communication Facilities (Ground / Rooftop)	C
Materials Storage	X
Light Manufacturing	X
Heavy Manufacturing	X
Warehouse/Distribution	X
Automobile Related	
Parking Facility, Public or Private ^{5, 6}	P
Service Station	C
<ol style="list-style-type: none"> 1. Must be in tandem with a sit down restaurant. 2. Alcoholic beverage sales establishments are subject to Chapter 31-50 of the Municipal code. Restaurants (bona fide eating places whose predominant function is the service of a full range of food selections in a sit-down setting with table service and little, if any emphases on 'take out' food with onsite accessory sales of alcohol) may incidentally serve alcoholic beverages with meals without need of a Conditional Use permit. 3. Outdoor dining is subject to the standards described in Section 4.6.4. 4. Only permitted in Single Family Homes. See Zoning Code Section 30-11.3. 5. Parking structures shall comply with the standards in Subsection 5.15.6. 6. When not a part of a larger development, otherwise permitted. 7. Artisan manufacturing shall consist of all kinds of craftsman and light manufacturing uses, including welding, of less than 10,000 square feet. 	

Figure 3-16. Permitted Uses, Urban Flexible (UF) Zone.

Development Standards – Urban Flexible (UF)

3.7.4 Height		
Minimum	None	
Maximum	2 floors, or 4 floors for residential	
3.7.5 Setbacks		
Front Setbacks	0 ft. min., 10 ft. max.	
Street Side Setback	5 ft. min., 10 ft. max.	
Interior Side Setback	0 ft. min., 10 ft. min. adjacent to residential.	
Rear Setback	10 ft.	
3.7.6 Interior Lot Line Height Transitions		
Adjacent to Residential Use	45-degree plane starting 25' high at property line	
Adjacent to Residential Use with Concession	60-degree plane starting 25' high at property line	
3.7.7 Minimum Residential Unit Size		
Single Room Occupancy	200 sf	
Studio	300 sf	
1-bedroom	500 sf	
2-bedroom	700 sf	
3-bedroom	900 sf	
3.7.8 Residential Open Space		
Common open space (for residents)	15% min. of site area.	
Open space required per unit	150 sf and 30% attached to unit.	
3.7.9 Minimum Parking Ratios		
	Unrestricted	Affordable
1+ bedroom units	1	0.5
SRO/studio units	0.5	None
Senior Housing (any size)	0.5	0.5
Guest Parking	0.1	0.1
Commercial Office	3/1,000 sf	
Retail or Restaurant	3.5/1,000 sf	
Other uses	30% less than CMC Table 30-21.A	

Figure 3-17. Development Standards – Urban Flexible (UF).

4. Height.

- A. Minimum height. There is no minimum height for new construction.
- B. Maximum height. All new construction shall have a maximum height of two stories, with the exception of four stories for residential new construction.
- C. Architectural projections, towers and elevator shafts may exceed the vertical height limit to the zone by 15 feet, as long as they are no longer than 30 feet long on any side.
- D. Mechanical equipment may exceed the vertical height limit, provided that it is not visible from an adjacent street, public right-of-way, or ground level private property.
- E. Roof decks, gardens and recreational amenities are permitted on the top floor of developments.

5. Setbacks, Projections, and Encroachments.

- A. Front Setbacks. No front setback is required. Buildings must also not be set back more than ten feet.
- B. Street side setback. Where the side of a lot abuts a public street, the building shall be set back no less than 5 feet and no more than 10 feet. Side frontages shall comply with frontage standards in Section 3.3.10.
- C. Interior Side Setback. No interior side setback is required. Buildings must also not be set back less than ten feet, if the building is adjacent to residential.
- D. Rear Setback. The minimum rear setback is ten feet.
- E. Portions of buildings and structures including marquees, awnings, shade structures, signs, and architectural features may encroach into the public right-of-way subject to encroachment permitting requirements of the City Zoning Code, and/or the review and approval by the City Engineer, or designee. Encroachments or projections into the right-of-way shall not include habitable space.
- F. No projection shall be less than 9 feet above sidewalk grade, except for A-frame signage.
- G. Projections into applicable setback areas are permitted as described in Figure 3-18 (Projections into Setback Areas). No projection shall be over a roadway or alley (sidewalk exempted).

Projections into Setback Areas				
Projection	Front	Rear	Side	Street Side
Arcade	10	N	N	10
Architectural Features	3	3	3	3
Balcony	7	6	6	6
Gallery	10	N	N	10
Shopfront (awning, bay window, or projecting shade structure)	10	N	N	10
Signage	P	N	N	P
Stoop	5	N	N	5
Terrace	6	N	N	6

Figure 3-18. Projections into Setback Areas.

- H. Awnings shall meet the following minimum standards:
 - I. Awnings shall be constructed of metal, canvas, or fire-resistant acrylic. Use of plastic and vinyl is prohibited.
 - II. Back-lit and waterfall awnings are prohibited.
 - III. Dome awnings are permitted only above building entryways. Only the address number or building name may be printed on the awning.

6. Interior Lot Line Height Transitions.

- A. Wherever an interior lot line abuts a single-family or multi-family residential building built before original adoption of this Specific Plan, new development shall not exceed the height established by an imaginary plane beginning 25 feet above the property line and rising up at a 45-degree angle toward the interior of the new development site.
- B. When a project utilizes a Public Benefit Concession to increase the transitional height pursuant to Section 3.12, wherever an interior lot line abuts a residential use built before original adoption of this Specific Plan, new development shall not exceed the height established by an imaginary plane beginning 25 feet above the property line and rising up at a 60-degree angle toward the interior of the new development site.



Required interior lot line height transition under base (left) and public benefit concession (right) standards.

7. Minimum Unit Size.

- A. The minimum residential unit size shall be 200 square feet for single-room occupancy units, 300 square feet for studio units, 500 square feet for one-bedroom units, 700 square feet for two-bedroom units and 900 square feet for three-bedroom units.

8. Residential Open Space.

- A. All projects containing a residential component shall include a common open space for residents, or series of common open spaces, no smaller than 15% of the lot area. This common open space may be located on top of roofs, podium parking garages, balconies, or commercial establishments if desired.
- B. A minimum of 150 square feet of open space shall be provided for each residential unit on-site. This requirement may be met through private or common open spaces. Up to 20% of the requirement may be met through publicly accessible on-site open space. A minimum of 30% of this requirement shall be met through private open space which is attached to the unit, such as balconies or private patios.

9. Minimum Parking Ratios.

- A. The minimum number of parking spaces required is given in Figure 3-17.
- B. Affordable residential unit parking ratios apply only to units which have a recorded

affordability covenant to serve households making 80% of area median income or less for at least 30 years.

10. Frontage Types.

- A. Buildings in the Urban Flexible zone are not subject to frontage type standards from other zones but are subject to the Standards for Active Ground Floor Design (Section 3.9).

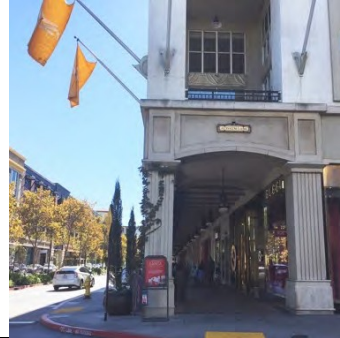
3.8 Frontage Standards

Figures 3-19 to 3-26 give standards for the allowable frontage types.

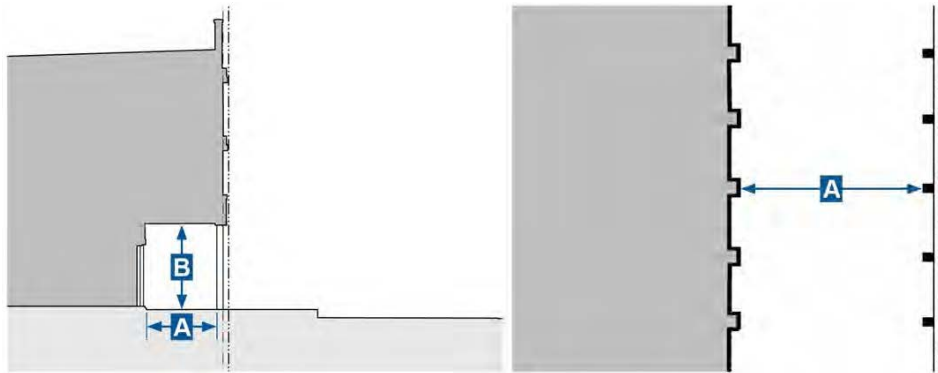
Arcade

Description

A covered walkway located entirely on private property with habitable space above. This frontage type provides adequate protection from the elements (e.g. shade) and circulation space for pedestrians and is intended for buildings with active ground floor uses and along public open spaces and enhanced pedestrian streets.



Standards



Depth, Clear (A)

10 ft min.

Ground Floor Height, Clear (B)

14 ft min.

Miscellaneous

The ground-floor façade of the building, under the arcade, shall be a shopfront and comply with all standards of the shopfront frontage type, except as these may conflict with the standards noted here for the arcade frontage type.

Shall have consistent depth along the frontage.

Figure 3-19. Arcade Frontage Standards.

Gallery

Description

The main façade of the building is near the right-of-way and the projecting gallery element encroaches into the sidewalk of the right-of-way. This frontage type is intended for buildings with active ground floor uses and may be one or two stories.



Standards



Depth, Clear (A)	8 ft min.
Ground Floor Height, Clear (B)	11 ft min.
Upper Floor Height, Clear (C)	9 ft min.
Height (D)	3 stories max.
Setback from Curb (E)	4 ft min.

Miscellaneous

The ground-floor façade of the building, under the gallery, may be either a shopfront or a lobby frontage type. It shall comply with all standards of the chosen frontage type, except as these may conflict with the standards noted here for the gallery frontage type.

Shall have consistent minimum depth along the frontage.

May project over the sidewalk, with an encroachment permit and maintain 4 ft of clear sidewalk abutting the curb face. Restrictions on blocking pedestrian access apply.

Figure 3-20. Gallery Frontage Standards.

Terrace

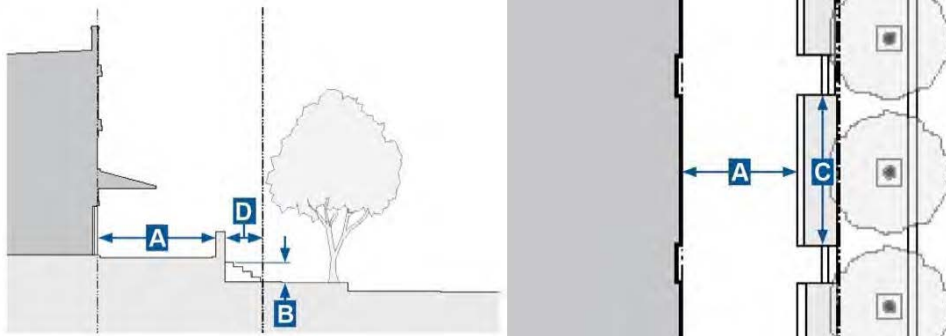
Description

The main façade of the building has an elevated terrace that projects outward and engages the sidewalk with frequent stairs. This frontage type allows at-grade access to elevated ground floor uses due to natural grade constraints or by artificially elevating the terrace floor. This frontage type acts as a buffer from the sidewalk and may be landscaped to provide additional privacy.

Building activities are slightly separated from the adjacent sidewalk by the terrace finish level, and the depth of the terrace provides a buffer for outdoor seating, private yards, shade, or any other appropriate uses.



Standards



Depth, Clear (A)	8 ft min.
Length	120 ft max.
Finish Level Above Sidewalk (B)	4 ft max.
Distance Between Stairs (C)	50 ft max.
Wall Setback from Right-of-Way (D)	5 ft min.
Transparency, Ground Floor	75% min.

Miscellaneous

The ground-floor façade of the building, along the terrace, may be either a shopfront or a lobby frontage type. It shall comply with all standards of the chosen frontage type, except as these may conflict with the standards noted here for the terrace frontage type.

Reasonable accommodation shall be provided as appropriate.

Figure 3-21. Terrace Frontage Standards.

Shopfront

Description

The main façade of the building is near the right-of-way with an at-grade entrance along the sidewalk. This frontage type is intended for active ground floor uses and has substantial glazing at the ground level.



Standards



Depth, Recessed Entries ¹	5 ft max.
Transparency, Ground Floor	60% min.
Distance Between Windows (A)	20 ft max.
Maximum Distance Between Entries	75 ft max.

Projection ²

Depth (B)	6 ft min.
Setback from Curb (C)	2 ft min.
Height (D)	9 ft min.

Miscellaneous

May be used in conjunction with another permitted frontage type (e.g., arcade, terrace). In case of a conflict between them, the other frontage standards supersede. Non-transparent or semi-transparent must include a bulkhead between 18 and 36 inches tall.

End Notes:

¹ May be designed in a variety of configurations.

² Only allowed for non-habitable space.

Parking Frontage

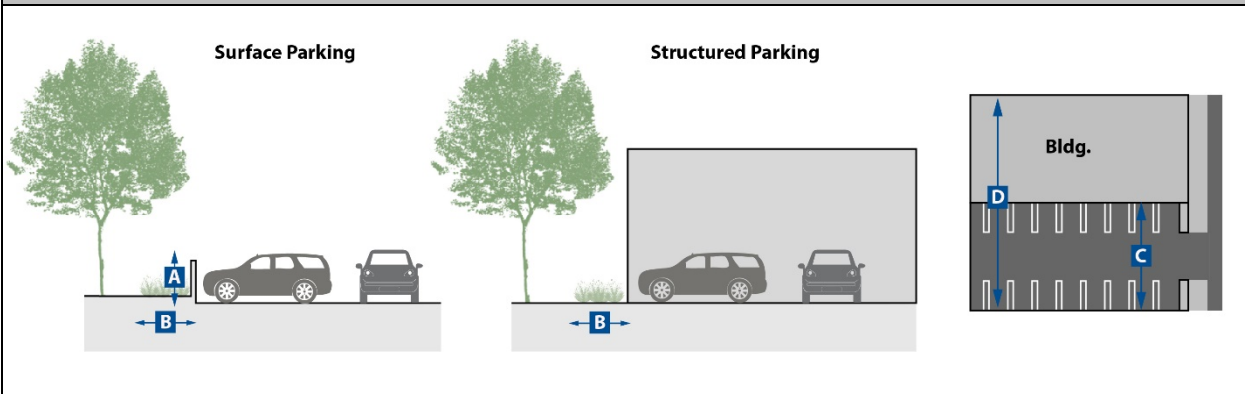
Description

Parking may be supplied on the surface or in subterranean, podium or above-ground structured configurations. Where appropriate, a limited amount of the street frontage may be used for surface parking. In these cases, the parking is screened by a wall and landscaping.

Parking along the street frontage may also be structured under this frontage type, subject to the conditions given below.



Standards



Parking Facility Width (C) as a function of Lot Width (D)

40% max, up to 200 ft.

Depth of Landscaping (B)

5' min, 10' max.

Wall Height (A)

4 ft max.

Transparency, Parking Structure Ground Floor

40% min.

Miscellaneous

Walls shall be opaque and made of masonry.

Use of a raised landscape planter, filter strip, infiltration trench or other stormwater best management practice is encouraged over raised landscape planters.

Structured parking garage frontages shall be screened or enhanced as described in Section 4.5.

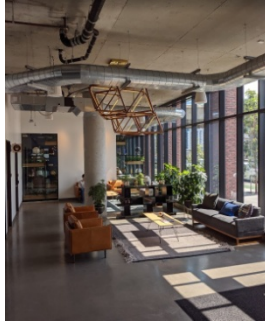
Where this frontage type is not permitted, surface parking may not be located along the frontage of the project. Only a driveway accessing parking located behind another building on the site is permitted.

Figures 3-22 and 3-23. Shopfront and Parking Frontage Standards.

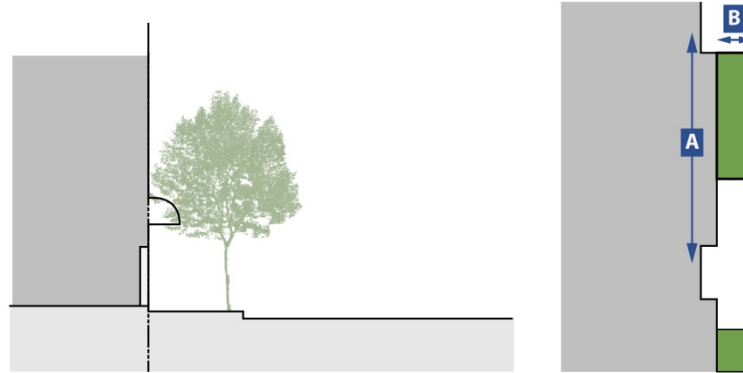
Lobby

Description

This frontage type is intended for office or residential uses which are accessed through a primary building entry located directly on the sidewalk. In office developments, common space areas are oriented toward the street in order to ensure compatibility with a transparent frontage.



Standards



Depth, Recessed Entries	4 ft min.; 8 ft max.
Optional Planting Width (B)	0 ft min., 4 ft max. On Compton Blvd, any frontage landscaping must be located in a planter.
Transparency, Ground Floor	60% (Compton Walk) 55% (Other, Commercial) 40% (Other, Residential)

Miscellaneous

Optional Planting width may be filled with movable planter boxes. On all frontages except Compton Blvd., planting width may also be filled with low landscaping.

Where residential frontage uses are allowed, private balconies may be located on the primary frontage if setback requirements permit.

Planter boxes and landscaping may not block any of the required transparency of the frontage, above three feet height.

Figure 3-24. Lobby Frontage Standards.

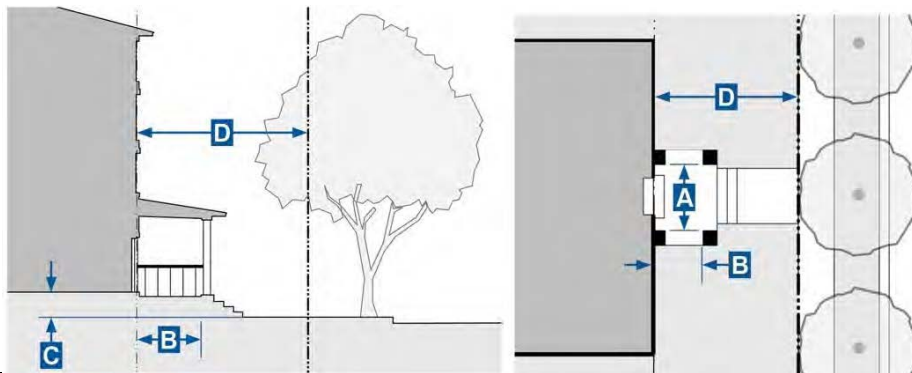
Stoop/Porch

Description

The main façade of the building is near the right-of-way and the stoop engages the sidewalk. Larger stoops can provide important residential open space and “eyes on the street.” This frontage type is elevated above the sidewalk to ensure privacy within the building. The entrance is usually an exterior stair and landing.



Standards



Width, Clear (A)	6 ft min.; 16 ft max.
Depth, Clear (B)	4 ft min.; 8 ft max.
Depth, Recessed Entries	6 ft max.
Finish Level above Sidewalk (C)	2 ft min.; 6 ft max.
Building Setback from Right-of-Way (D)	As prescribed by the development standards applicable to the zone. However, maximum setback may be exceeded by up to 5 ft.
Stoop/Porch Setback from Right-of-Way (E)	3 ft min. Maximum is the maximum prescribed by the zone, but not greater than 15 ft.

Miscellaneous

Reasonable accommodation shall be provided as appropriate.

Stairs may be perpendicular or parallel to the building façade.

Entry doors shall be covered or recessed.

All semi-subterranean parking areas shall be hidden with landscaping.

Figure 3-25. Stoop/Porch Frontage Standards.


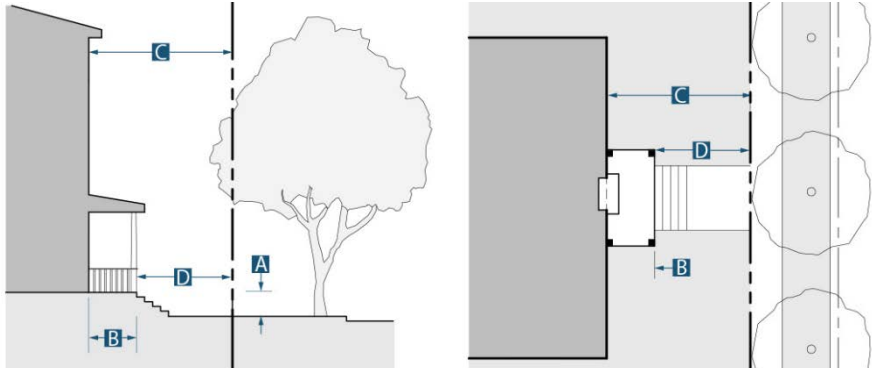
Front Yard	
Description	
<p>The yard frontage type corresponds to residential buildings whose flat, landscaped area serves as a transition between the public and private realm. It is typically used to emphasize commonality with the design of existing lower-density residential neighborhoods and is especially appropriate on local streets occupied by single-family homes, with a yard depth similar to the prevailing design.</p> <p>Low fences and porches are optional elements which may be added to residential buildings with yard frontages.</p>	
Standards	
	
Fence Height, Level above Sidewalk (A)	3 ft max.
Porch Depth, Clear (B)	6 ft min.
Building Setback from Right-of-Way (C)	15 ft min, max can exceed zone max. by 2 ft if porch is used
Stoop/Porch Setback from Right-of-Way (D)	10 ft min.
Miscellaneous	
The use of fences and porches are optional.	
The use of drought – tolerant ground coverings are highly encouraged.	
A primary pedestrian pathway shall be provided directly from the sidewalk to the primary entrance of the structure.	

Figure 3-26. Front Yard Frontage Standards.

3.9 Standards for Active Ground Floor Design

- A. The main entrance or entrances shall be emphasized and oriented to the street or major plazas or open space.
- B. Secondary entries may be from parking areas, the rear of the building, or interior open spaces.
- C. In mixed use projects, when residential and commercial uses are combined in the same structure, separate entrances shall be provided for each use. Residential entrances may be designed as follows:



Separate entrance on sites with two or more street exposures.



Integrated with retail façade.



Access from alleyways and courtyards

- D. When a building entrance is located at the intersection of two public streets, at least one of the following shall be provided:
 - I. Two primary entrances, one facing each street; or
 - II. A corner entrance, oriented toward both streets placed at an angle of 45 degrees.
- E. The main entrance to a building that is open to the public shall be clearly identifiable by emphasizing and enhancing the architectural details, such as a change in plane (e.g., the entrance may be recessed on the street level façade), differentiation in material and color, enhanced lighting, and signage.
- F. Utility, mechanical room, or service entrance doors shall not face any street frontage.
- G. Within the Downtown Core (DC) and Downtown Transition (DT) zones, any building or development with 200 linear feet or more of street frontage and containing nonresidential uses shall provide pedestrian access into the site by means of a pocket plaza or other civic space of at least 600 square feet and with a minimum dimension of 25 feet.
- H. Each development shall include no more than one vehicular access point per 200 linear feet of street frontage. On-site loading areas may be accessed separately, but shall include no more than one per land use, and not be visible from the street.

3.10 Public Art Requirements

- A. Purpose. The design of buildings in Downtown Compton should express the unique culture of the City. As a result, public art should be included in each project.
- B. Any project which is subject to design review and has a valuation which equals or exceeds one million dollars (\$1,000,000), shall provide public art valued at 0.5% of the project's total valuation
- C. For the purposes of this Section, project valuation shall be the valuation of the building or structure as determined by the Building Division for the issuance of the building permit(s).
- D. This public art shall be located outdoors and be visible from a public street.
- E. A preliminary description of the artwork shall be submitted with the application for a design review permit. All conceptual designs for public art shall be submitted with the project plans to be approved with the initial design review or CUP. Within the conceptual design submission should be details regarding the type and theme of the public art.

3.11 Inclusionary Zoning Requirement

- A. All residential projects containing more than twenty (20) residential units shall demonstrate that a minimum of 5% of units are deed-restricted affordable housing secured by covenants of at least 55 years for low-, very-low or extremely-low income households.

3.12 Public Benefit Concessions Menu

- A. Purpose. The purpose of this section is to incentivize the provision of development features that create public benefits but may not of themselves generate sufficient income to be financially feasible. This section provides a menu of by-right concessions that can be requested by the applicant in exchange for the provision of some of these benefits.
- B. Relationship to State Density Bonus Law. An applicant who seeks incentives per the following shall not take advantage of the concessions, waivers or other incentives described in CMC 30-47.5 or other concessions in State Density Bonus Law.
- C. Public Benefits. Concessions shall be granted for the following public benefits:
 - I. Affordable Housing. Deed-restricted affordable housing units secured by covenants of at least 55 years for:
 - a. 3% extremely low income, 6% very low income, 15% low income or 25% moderate income units: one concession
 - b. 6% extremely low income, 10% very low income, 20% low income or 40% moderate income units: two concessions
 - c. 100% affordable housing with maximum income levels of 80% low income and 20% moderate income units: four concessions. Per Section 3.2.7.D, these projects are not subject to Tier 2 Design Review.
- D. Publicly Accessible Open Space. Privately maintained open space that is open to the general public for use from at least 6:00 am to 10:00 pm every day. The open space is required to be monitored by 24-hour security. Said open space(s) must cover at least 3% of the project site. Each countable plaza must have a minimum dimension of 25 feet: one half concession. Concessions shall not apply to any open space that is required.
- E. Public Gathering Space. Direct provision of a library, community meeting room or other space which is open for public use and gatherings without charge and is of at least 500 square feet in size: one half concession
- F. Offsite Streetscape Amenities. Installation of landscaping, trees, benches, curb work,

bike lanes, etc. described in this Plan which are not located on the public right-of-way immediately abutting the project site: one half concession per 1% of project cost invested, to a maximum of one concessions

- G. Public Parking. Parking spaces signed and managed as public parking that are in addition to the requirements of this chapter, at least 10% of total project parking and at least 20 spaces: one concession
- H. Provide a 24 hour onsite shuttle bus for residents: two concessions.
- K. Concessions. The following concessions are available:
 - I. Height. Each concession is worth a one-story bonus over 50% of the building footprint. Maximum is two stories greater than the base height limit.
 - II. Interior Lot Line Height Transitions as described in Sections 3.3 through 3.7.
 - III. Elimination of on-site loading requirements
 - IV. Count on-street parking spaces as part of parking requirement.

3.13 Signage Standards

1. Applicability

- A. All signs within the Specific Plan area shall be designed, constructed, and installed in compliance with the standards established in Section 30-22 (Sign Regulations) of the City Zoning Code.
- B. Additional standards for signs permitted within the Specific Plan area are included in Subsection G.2 (Additional Specific Sign Standards) below. In the case of a conflict between the sign standards established in this Chapter and the standards in Section 30-22 (Sign Regulations) or any other sign standards included in the City Zoning Code, the standards in this Chapter supersede.
- C. Regulations should apply to Downtown Core, Downtown Transition, and Neighborhood Corridor zones only. Elsewhere citywide standards apply.

2. Sign Illumination Standards

- A. Direct Illumination. Direct illumination is limited to marquee signs; see Subsection 2.a (Marquee Signs).
 - I. Direct illumination is limited to letters, numbers, symbols, and accents on the marquee sign.
 - II. Exposed lamps on marquee may only be animated to create an effect of patterned illusionary movement provided the alternate or sequential activation of illuminated elements occurs on a cycle that exceeds two seconds.
 - III. Hours Permitted. All direct illumination on a marquee sign must be turned off daily at the close of business.
- B. Neon Signs
 - I. Exposed neon sign lighting is permitted only for entertainment or restaurant uses.
 - II. Exposed neon sign lighting is considered a building design element for nighttime entertainment uses and restaurants. Signs in neon are subject to the sign codes in section 30-22.
 - III. Hours Permitted. Exposed neon sign lighting must be turned off daily at the close of business.
- C. Electronic Changeable Message Signs
 - I. Number of Signs. One electronic LED message sign is allowed per business and will be treated as the primary sign.
 - II. Changeable LED Copy Sign.
 - a. Applicable Uses. Changeable LED copy signs are permitted for

restaurants and entertainment uses. Other uses may use changeable LED copy signs with a Minor Use Permit.

b. **Display.** Electronic message signs must not flash, blink, flutter, include intermittent or chasing lights, or display video messages (i.e., any illumination or message that is in motion or appears to be in motion). Electronic message signs may display changing messages in addition to the business name provided that each message is displayed for no less than 8 seconds.

III. **Night-time Brightness.**

a. **Automatic Controls.** Electronic changeable copy message signs must be equipped with photocell technology to control and vary the intensity of light output depending on the amount of ambient light that is present to prevent overly bright luminance at night. Automatic controls must limit night luminance to a maximum of 100 nits when the display is set to show maximum brightness in 100 percent full white mode.

b. **Manufacturer Certification.** The applicant shall provide a written certification from the sign manufacturer that the nighttime luminance has been factory pre-set not to exceed 100 nits as described in paragraph 8.1.2.C.1. above, and that this setting is protected from end-user modification by password-protected software or other method as deemed appropriate by the Community Development Department Director.

c. **Hours Permitted.** Electronic message signs must be turned off daily at the close of business.

3. Sign Program

A. **Purpose.** The purpose of the sign program shall be to integrate signs with building and landscaping design to form a unified architectural theme which conforms to the goals of this Specific Plan and the city's General Plan.

B. **Applicability.** A sign program shall be required for the following projects:

I. New or substantially rehabilitated commercial, mixed-use, office and industrial projects with two or more tenant spaces.

II. Changeable copy signs.

C. **Review Authority.** The sign program shall be submitted and approved as part of the applicable design review procedure.

D. **Design Guidelines.** The purpose shall be achieved by using the same background colors and limiting the number of sign colors per site; using the same type of cabinet supports, mounting methods, component construction materials and illumination; and using uniform sign placement and proportionate letter and logo heights for large and small tenants.

E. **Flexibility.** After consultation with the Architectural Review Board, the Community Development Department Director may modify any sign standard(s) by up to 15% provided that the Director find that the modification in standards will not be detrimental to the design of the project or the experience of pedestrians along the sidewalk abutting the project.

4. Definitions

A. **A-FRAME SIGN** - Shall mean a freestanding, moveable sign usually supported by two upright sign faces (also known as a "sandwich board").

B. **ABANDONED SIGN** - Shall mean a sign whose use has ceased or been discontinued for a period of 90 consecutive days or which identifies a business or activity that has not occupied the premises for a period of 90 consecutive days.

C. **ACCESSORY SIGN** - Shall mean a sign whose copy refers to the products, facilities, or

services available on the premises.

D. ADDRESS - Shall mean the placement of a street number which identifies the street mailing address of a business or residence. It may also include the street name for additional clarity.

E. ADVERTISING STATUARY - Shall mean a three-dimensional imitation or representation of a person or thing which is designed to promote or represent a commercial enterprise.

F. ADVERTISING STRUCTURE - Shall mean a structure of any kind or character erected, used, or maintained for outdoor advertising purposes, upon which any poster, bill, printing, painting, or other advertisement of any kind whatsoever may be placed.

G. ANIMATED OR MOVING SIGN - Shall mean a sign which uses movement, lighting, or special materials to depict action, movement, or rotation.

H. APPROVING AUTHORITY - Shall mean the administrative, appointed or legislative agent or body with primary responsibility for approval of a sign permit.

I. ARCHITECTURAL REVIEW BOARD - Shall mean an interdepartmental board established by the City to review and evaluate new projects to ensure that they meet the highest standards for design and construction.

J. AUDIO - Shall mean the inclusion in a sign of any sound emitting mechanism or production of sound from a sign.

K. AWNING OR CANOPY SIGN - Shall mean a sign that is mounted or painted on or attached to the vertical surface or flap of an awning or canopy.

L. BANNER SIGN - Shall mean a nonpermanent sign composed of fabric, plastic, paper, or other lightweight material which contains advertising copy and is attached to a building, pole, frame, or vehicle.

M. BENCH/TRANSIT ENCLOSURE SIGN - Shall mean a sign displayed on a transit bench or on any surface of a transit enclosure.

N. BILLBOARD SIGN - Shall mean a sign, excluding a supergraphic sign, which advertises goods, products, services, or facilities not sold, produced, manufactured, or furnished on the premises on which the sign is located (also known as outdoor advertising, off-premises sign or off-site sign).

O. BUILDING FACE OR FRONTAGE - Shall mean that portion of a main building most nearly parallel to a street or parking area.

P. BUSINESS IDENTIFICATION SIGN - Shall mean sign copy, including logo, used to identify the name and address of a premises, business, building or portion of building upon which it is located, and which includes no other information.

Q. BUSINESS INFORMATION SIGN - Shall mean a sign in which a business or occupant's name, address, phone number or hours of operation are provided, but which contains no advertising copy.

R. CANBINET SIGN - Shall mean a sign contained within a structural casing or canister, often composed of sheet metal or aluminum, covered by glass, plastic, or similar material upon which the sign copy is affixed, and which may be internally illuminated (also known as a "canister sign").

S. CANOPY SIGN - See "awning sign."

T. CENTER IDENTIFICATION SIGN - Shall mean a sign containing the name and/or primary occupants of a multi-tenant center (also known as a "multi-tenant sign").

U. CHANGEABLE COPY SIGN - Shall mean a sign in which the copy/text may be manually changed or rearranged without altering the face or surface of the sign.

V. CHANNEL LETTER - Shall mean three dimensional individual letters or figures affixed to a building, raceway, or other sign structure.

W. CIVIC EVENT SIGN - Shall mean a temporary noncommercial sign, posted to advertise a

civic event sponsored by a public agency, school, church, civic-fraternal organization, or similar civic organization.

X. **CONDITIONAL USE PERMIT** - Shall mean a permit approved by the Planning Commission at an advertised public hearing for specified types of signs which are not automatically permitted by the Code.

Y. **CONSTRUCTION SIGN** - Shall mean a temporary sign erected on a parcel where construction is taking place, limited to the duration of the construction, indicating the names of the architects, engineers, contractors, owners, financial supporters, sponsors and similar individuals or firms having a major role or interest in the project.

Z. **COPY AREA** - Shall mean the actual area of sign copy applied to any background including any lettered, written, pictorial or other visual sign unit displayed for the purpose of conveying a message.

AA. **DIRECTIONAL SIGN** - Shall mean an on-premises sign limited to directional messages principally to facilitate safe movement of pedestrian or vehicular traffic, such as "one-way," "entrance" or "exit."

BB. **DIRECTORY SIGN** - Shall mean a sign listing the tenants or occupants of a building or group of buildings which may contain a map, suite numbers, names, and logos but no advertising copy.

CC. **DOUBLE-FACED SIGN** - Shall mean a single sign structure with copy on both sides.

DD. **EAVELINE** - Shall mean the bottom of the roof eave or parapet.

EE. **ELECTRICAL SIGN** - Shall mean a sign or sign structure in which electrical wiring, connections or fixtures are used.

FF. **ELECTRONIC READERBOARD** - Shall mean a changeable message sign consisting of a matrix of lamps which are computer controlled (excluding time/temperature signs).

GG. **EMERGENCY SIGN** - Shall mean a sign placed temporarily by or with permission of a public safety agency to protect life and property in the case of fire, flood, explosion, or other threat to public safety.

HH. **FACADE** - Shall mean visible exterior building walls, including parapet walls.

II. **FACING** - Shall mean the portion of the advertising structure that contains any poster, bill, printing, painting, or other advertisement of any kind whatsoever.

JJ. **FASCIA** - Shall mean a horizontal band covering the joint between the top of a wall and the projecting eaves.

KK. **FLAGS, FESTOONS, SPINNERS, AND PENNANTS** - Shall mean ribbons, tinsel, small flags, pinwheels, pennants, small balloons, and similar items composed of fabric, plastic, paper, or other light materials which are mounted to allow movement by the atmosphere and to attract the attention of the public (excluding official government flags). Inflatable signs are prohibited.

LL. **FLASHING, INTERMITTENT OR MOVING LIGHT** - Shall mean a light or message that changes more often than once every four seconds. The illumination or the appearance of illumination resulting in a change of message or advertising copy is not the use of flashing, intermittent, or moving light, unless it changes more often than once every four seconds.

MM. **FREESTANDING SIGN** - Shall mean a permanent sign not affixed to a building, including ground signs, pole signs, pylon signs and monument signs.

FUTURE FACILITY/BUSINESS SIGN

NN. **GRAND OPENING** - Shall mean a promotional activity not exceeding 30 consecutive calendar days which occurs within two months of the establishment of a new business to inform the public of its location and product or service. Grand openings do not include annual or occasional promotions.

OO. **GROUND SIGN** - Shall mean a very low profile, freestanding sign oriented parallel to the

ground, which maintains essentially the same height throughout its length.

PP. HEIGHT - Shall mean the vertical distance from the natural grade beneath a sign measured to the highest point of the sign, including embellishments.

QQ. ICON SIGN - Shall mean a sign designed to resemble the product or service (e.g., donuts, keys, or shoes) offered on the premises.

RR. ILLEGAL SIGN - Shall mean a sign, advertising display or structure erected or constructed without first complying with all ordinances and regulations in effect at the time of its construction, erection, or use; a nonconforming sign which has exceeded its authorized amortization period; an abandoned sign or any sign which is hazardous due to its location, physical condition, or lack of required maintenance.

SS. ILLUMINATED SIGN - Shall mean a sign lighted by or exposed to artificial lighting either by lights on or in the sign or directed toward the sign.

TT. INCIDENTAL SIGN - Shall mean a small sign, emblem or decal providing information to the public regarding services available on the premises, such as credit cards, ATM availability, self-service, cashier, etc.

UU. INFLATABLE SIGN - Shall mean inflatable statuary, hot air balloons, captive balloons or similar three dimensional inflatable objects designed to convey information or advertise a location and mounted to allow movement by the atmosphere and to attract the attention of the public.

VV. KIOSK - Shall mean a vertically oriented sign, typically with multiple faces, which is intended primarily to provide information to pedestrians.

WW. LEGALLY NONCONFORMING SIGN - Shall mean a sign which was legally established but no longer conforms to the provisions of this section.

XX. LOGO SIGN - Shall mean a licensed, certified, copyrighted, or trademarked name, symbol, feature, or trademark that represents a business, enterprise, group or activity.

YY. MAIN TRAVELED WAY - Shall mean in the case of a freeway, the traveled way of each of the separate roadways for traffic in opposite directions.

ZZ. MARQUEE - Shall mean a sign located on a fixed, roofed structure which projects from a building and which is designed to advertise changing performances, attractions, or events, either through manually placed copy or electronically controlled lighting. Information presented may be static or sequential, depending on the design of the copy system.

AAA. DRIVE THRU MENU BOARD - Shall mean a sign on the premises of a drive-thru restaurant which identifies the type and cost of available food items.

BBB. MONUMENT SIGN - Shall mean a low profile, freestanding sign with a solid or primarily solid support base.

CCC. MULTI-FACED SIGN - Shall mean a sign containing three or more faces.

DDD. MULTI-TENANT SIGN - Shall mean a sign which identifies or advertises more than one business or activity within a single sign structure (also known as a "center identification" sign).

EEE. MURAL - Shall mean a display painted onto a wall or display base which is designed and intended as a noncommercial decorative or ornamental feature and which does not contain any advertising copy (also known as "decorative graphic").

FFF. NAMEPLATE - Shall mean a non-illuminated, on-premises sign which gives only the name, address and/or occupation of an occupant or group of occupants.

GGG. NEON SIGN - Shall mean a sign which utilizes neon or other gases with translucent tubing in or on any part of the sign structure.

HHH. PAINTED OR PAINTED-ON SIGN - Shall mean a sign which is applied with paint or a similar coating directly on the surface of a wall, building, display base or sign structure.

III. POLE SIGN - Shall mean a freestanding sign mounted above the ground on a single pole

structure, typically centered on the pole (excludes directional signs six feet or less in height).

JJJ. POLITICAL ELECTION SIGN - Shall mean a temporary sign used in connection with a local, State, or national election or referendum.

KKK. PORTABLE SIGN - Shall mean a moveable sign which is designed to be easily moved and is not permanently attached to the ground or a building. Portable signs include A-frame signs, portable reader-boards, and similar signs.

LLL. PROJECTING SIGN - Shall mean a sign which is attached to and projects from the exterior wall of a building where the display surface of the sign is not parallel to the wall of the structure to which it is attached.

MMM. PROMOTIONAL ACTIVITY SIGN - Shall mean a sign erected on a temporary basis to promote the sale of new products, new management, new hours of operation, a new service, or a special sale.

NNN. PYLON SIGN, DOUBLE - Shall mean a freestanding sign which is designed in a "gateway" format, with a double support structure and a connecting, thematically integrated panel on which is typically placed copy and/or a logo to mark the name identification of a major business, center, or facility. Double pylon signs are architectural or decorative in character to emulate gateways.

OOO. PYLON SIGN, SINGLE - Shall mean a freestanding sign which is incorporated into a single, vertical structure on which is typically placed copy and/or a logo to mark the name identification of a major business, center, or facility. Single pylon signs are decorative in character to emulate the form of a monument.

PPP. RACEWAY - Shall mean a channel attached to a building which provides a mounting surface for a facade sign and which generally contains the electrical connections for illumination.

QQQ. REAL ESTATE SIGN - Shall mean a temporary sign that relates to the sale, lease or rental of property or buildings on the premises on which it is located.

RRR. ROOF SIGN - Shall mean a sign that is mounted on the roof of a building or that is dependent upon a building for support and/or that projects above the top walk or edge of a building with a flat roof, the eave line of a building with a gambrel, gable or hip roof or the deck line of a building with a mansard roof.

SSS. SIGN - Shall mean an object, device, display or structure, or part thereof, situated outdoors or indoors, which is used to advertise, identify, display, direct or attract attention to an object, person, institution, organization, business, product, service, event, or location by any means, including words, letters, figures, design, symbols, fixtures, colors, illumination or projected image.

TTT. SIGN FACE - Shall mean the exterior surface of a sign, exclusive of structural supports.

UUU. SIGN PERMIT - Shall mean a permit issued by the Department of Building and Planning for installation of a sign.

VVV. SIGN PROGRAM - Shall mean a comprehensive plan that addresses all signs within a site that is designed to achieve aesthetically appealing and compatible signage for projects with major roadway exposure, multiple-signage requirements or complex or unusual signage needs.

WWW. SIGN STRUCTURE - Shall mean the structural supports, uprights, bracing and decorative skirting for a sign.

XXX. SUPERGRAPHIC SIGN - Shall mean a sign, consisting of an image projected onto a wall or printed on vinyl mesh or other material with or without written text, supported and attached, to a wall with an adhesive, stranded cable and eye bolts or other materials or methods which covers more than 20% of a wall, building, facade, or other structure.

YYY. TEMPORARY SIGN - Shall mean a sign, generally constructed of paper, cloth, canvas,

or other similar lightweight material, with or without a frame, intended to be used for a limited period of time.

ZZZ. TIME/TEMPERATURE SIGN - Shall mean a sign which communicates time and temperature information in digital or analog fashion, frequently in connection with business logo or identification copy.

AAAA. UNDER CANOPY SIGN - Shall mean a sign suspended beneath a projecting canopy, awning, ceiling, or marquee.

BBBB. VEHICLE SIGN - Shall mean a sign which is attached to or painted on a vehicle the principal purpose of which is to attract attention to a product or provide directions to an activity or business.

CCCC. WALL SIGN - Shall mean a sign which is attached parallel to and projects no more than 12 inches from the exterior wall of a building, building facade, perimeter wall or fence.

DDDD. WINDOW SIGN - Shall mean a sign, picture, text, or symbol designed to communicate information about an activity, business, commodity, event, sale or service that is placed in or on, or is visible from, the exterior of a window.

Sign Matrix			
Sign Type	Maximum Number	Maximum Area	Other
Façade: Wall or Canopy			
Business or Center Identification	1 per street, paseo, or parking lot frontage	2 sf per lineal foot of frontage, not to exceed 200 sf	Maximum area applies to aggregate of all façade signs
Accessory	2 per street or parking lot frontage, with a maximum of 8 installed on no more than 2 facades	40% of the size of the business identification sign per accessory sign	Maximum 2 line copy, 1 product or service per sign May not project above height of primary sign or first floor
Canopy			Awning and canopy signs may be permitted only as an integral part of an awning or canopy to which they are attached or applied. Signs on awnings are only permitted on the awning flap (valance) and content is limited to the business name, building name or address number. The letter height shall not exceed 10 inches. The lettering shall be located in the middle 70 percent of the valance area.
Under Canopy	1 sign per primary business entrance	8 sf	
Free Standing Signs			
A-Frame	1 sign per business	6 sf per face	Maximum height: 3 ft Maximum width: 2 ft Must be located in Sidewalk Amenity Zone and must be removed each day at close of business.
Business Identification: Monument	1 sign for each street frontage of 100 feet or more; not to exceed 2	32 sf per face	Maximum height: 5 feet To be used in conjunction with the Surface Parking frontage type Not permitted on the Compton Walk Frontage
Center Identification: Monument	1 sign only per center with 200 feet or more of street frontage located on a single street	1 sf for each lineal foot of frontage, not to exceed 200 sf	Must have at least three tenants. Can be building wall mounted To be used in conjunction with the Surface Parking frontage type Not permitted within 40 feet of Compton Walk Frontage

Projecting Signs			
Small Business Identification: Projecting (Blade)	May be used in place of Under Awning sign 1 sign per primary business entrance	8 sf per face	
Large Business Identification: Grand Projecting	May be used in place of Business Identification: Monument	32 sf per face	Mounted perpendicular to building wall at front entrance.
Center Identification: Grand Projecting	May be used in place of Center Identification: Monument	0.5 sf for each lineal foot of frontage, not to exceed 200 sf	
Marquee	1 per theater (movie or live performance)	1 sf per linear foot of frontage for the specific use being advertised in the marquee	Bottom of marquee must be mounted 12 feet from sidewalk. Marquee may encroach over sidewalk, but no closer than 15 feet from the bike curb.
Special Purpose Signs			
Wall: Directory Signs/Multiple Tenant Centers	1 wall sign per building entrance or pedestrian walkway entrance	12 sf per sign face	Maximum height: 6 feet (Ground Mounted)
Kiosk: Directory Signs/Multiple Tenant Centers	1 kiosk per pedestrian walkway entrance or plaza	12 sf of written area per face	Maximum height: 7 feet May be located within Sidewalk Amenity Zone One face shall reserve 4 sf for City agendas and public notices
Directional/Wayfinding Signage (Pedestrian)	1 per every 50 feet of street or walkway frontage or at points where changes in direction are required	4 sf per face	Maximum height: 8 feet
Artistic Mural or Mural Wall Sign	1 per building elevation	150 sf	Non-illuminated only Content must be approved by Community Development Department Director

Figure 3-27. Sign Matrix.

Signs	
Sign Type	
A-Frame	P
Address	E
Audio	X
Balloons (helium filled, 1 foot diameter max.)	T
Banner (see CMC 30-22.5.c.4)	T
Bench (Transit)	X
Billboard	X
Business Identification	P
Canopy (Awning)	P
Center Identification	P
Construction	T
Directional	P
Directory	P
Emergency	E
Façade (Wall)	P
Flag (National)	E
Flags (Spinners, Festoons, etc.)	X
Flashing	X
Grand Projecting	P
Ground	X
Holiday Decorations	E
Incidental	E
Inflatable	X
Kiosk	P
Marquee	MUP
Menu	X
Monument	P
Mural	P
Painted-on	X
Pennant (Spinners, Festoons, etc.)	X
Pole	X
Political Election	T
Portable	P
Projecting	P
Pylon	X
Real Estate	T

Roof	X
Rotating	X
Supergraphic	MUP
Temporary	T
Time & Temperature	P
Under Canopy	P
Vehicle	X
Wall (Façade)	P
Window	X
E = Exempt	
P = Permitted	
MUP = Minor Use Permit	
T = Temporary Only	
X = Prohibited	

Figure 3-28. Permitted Signs.

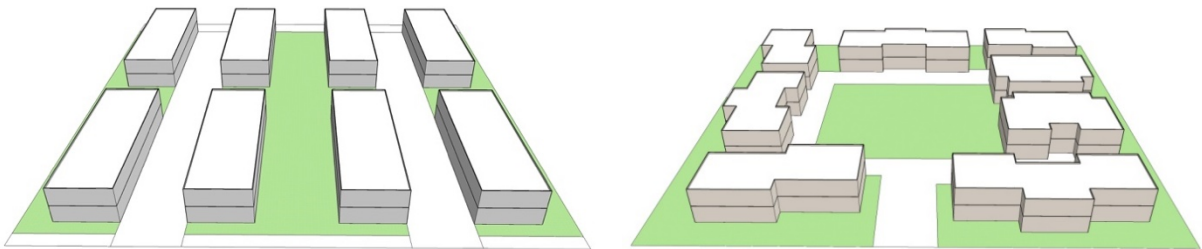
Chapter 4: Design Guidelines

4.1 Introduction and Use of Design Guidelines

1. The intent of the following guidelines is to ensure consistent development outcomes within the Specific Plan area that reflect the vision described in Chapter 2.
2. The chapter will be used as a guide for Design Review by the appropriate review bodies (Tier 1 – Community Development Department Director; Tier 2 – Planning Commission), described in Section 3.1, as well as for general project reviews by the Community Development Department. The appropriate review body should assess overall compliance with the guidelines as a part of project review; the assessment may impact project approval.
3. Guidelines below are organized by functional topics and apply to all multi-family residential, commercial, and mixed-use construction.

4.2 Building Siting and Orientation

1. Buildings should be located and designed to facilitate pedestrian activity by being oriented to the street and sidewalks.
2. Entry and edge design features such as landscaping, architectural signage and monumentation, and/or enhanced paving should be incorporated.
3. Buildings should be sited to avoid random and irregular building relationships, and arranged to create a sense of unity and overall harmony. To the maximum extent possible, new structures should be clustered to create plazas and open space areas and avoid creating of “barrack-like” rows of structures. Where clustering is impractical, a visual link between separate structures should be established. This link can be accomplished through the use of an arcade system, trellis, or other open structure.



Discouraged barrack-like layout (left) and encouraged orientation of buildings to create usable open space (right).

4.3 Building Modulation and Articulation

1. Building design should avoid large monotonous facades, long straight-line building fronts, plain box shapes, and barren exterior treatment. All building elevations visible from a public way should be highly articulated.
2. Building design should emphasize individual units within a building, larger units and/or anchor stores within retail projects, and foyers, lobbies, and reception areas within non-retail commercial projects. Ways to articulate buildings include giving individual identity to each vertical module by using design elements such as varying building heights within a project, providing a deep notch between the modules, varying architectural elements between units (e.g., window color, roof shape, window shape, stoop detail, railing type), providing porches/balconies, and varying colors/materials of each module within a harmonious palette of colors materials.
3. Building form and articulation should be used to accentuate public entrances, de-emphasize service areas, and define and shelter pedestrian walkways and exterior spaces.
4. Buildings with angled corners, plazas, or other architectural features are encouraged at corner locations to help anchor the intersection. Building corners may be emphasized by use of elements such as towers, domes, or entries. Vertical architectural elements such as towers should be used as focal points. Towers may exceed the vertical height limit to the zone by 15 feet, as long as they are no longer than 30 feet on a side and do not contain floor space. Additional flexibility in heights is available per the Administrative Relief procedure described in Figure 3-2.
5. The height of taller buildings should transition down toward lower buildings that are adjacent or across the street.



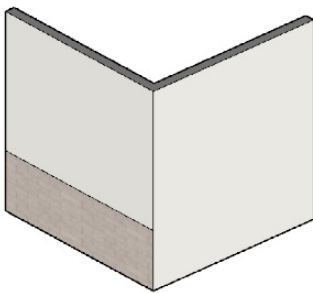
Vertical Articulation (left) and Horizontal Articulation (right).



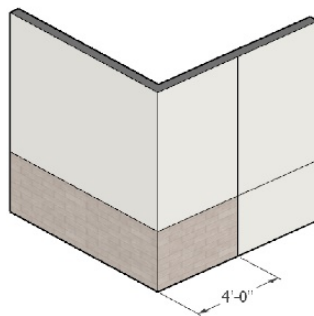
Height transitions between historic buildings (center) and new development (left and right).

4.4 Architectural Theme, Materials, Finishes, and Color

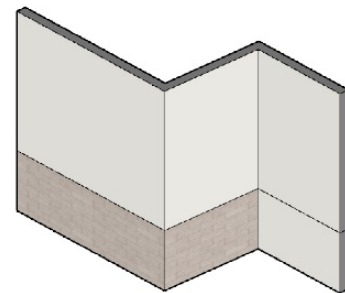
1. There is no required architectural theme for new development within the Specific Plan area. However, it is envisioned that larger developments will use contemporary styles. Original art murals may be used on primary or secondary facades, subject to the standards described in Section 3.13.
2. The architectural style and use of materials should be consistent throughout the entire development. Differences in materials and details may occur where the intent is to differentiate between the residential and commercial scale and character of the structure(s). High-quality, innovative, and imaginative architecture is encouraged.
3. For remodels or additions, the theme should be true to the original intent and style of the building.
4. High-quality materials and finishes appropriate to the architectural style, scale, character, and design theme of the building should be used. Materials, finishes and colors should unify a structure's appearance and be selected to work harmoniously with each other. Exterior design and building materials should reflect permanence and quality. Graffiti-resistant materials and paints are strongly encouraged.
5. Buildings should be treated as a whole and finished appropriately on all sides (360 degrees) to appear integral to the building and avoid appearing "tacked on".
6. Material changes shall not occur at building corners. The same material shall continue around corners for a minimum distance of ten feet. Ideally, the same material should continue to the next change in the wall plane.



Prohibited
Change of material at corner



Required - Option 1
Continue material around corner
for a minimum of 4 feet



Required - Option 2
Continue material around corner
to a change in wall plane

4.5 On-Site Circulation

1. Pedestrian walkways to connect buildings on a site to each other, to on-site automobile and bicycle parking areas, and to any on-site open space areas and amenities shall be provided.
2. All pedestrian walkways and publicly accessible areas shall be well-lit. Lighting should be low mounted and downward casting in a manner that reduces light trespass onto adjacent properties.
3. Where pedestrian paths or walkways cross parking areas or driveways, decorative and high-visibility paving and/or speed tables should be used to define the pedestrian space and delineate crossings.



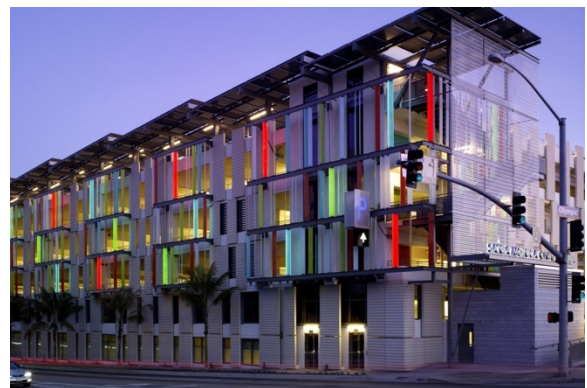
Walkway lighting (left) and speed table across local street connecting plazas and walkways (right).

4.6 Parking Facility Design

1. All public and commercial parking facilities should be well-signed.
 - A. Parking structures should in general be hidden behind buildings. In the case that structures are located on any street frontage, they are subject to the frontage standards in Figure 3-23.
 - B. When structures are standalone (not located within the podium of a building), they are subject to the design standards in Section 5.15.6.
 - C. The massing and architectural design of parking structures should be consistent and compatible with the rest of the development within which the parking structure is to be located. Heights shall be shorter than adjacent buildings on-site.
 - D. Parking structures should have openings for natural lighting and ventilation and demonstrate a unified aesthetic on the exterior façade using complete or half-height trailing, spandrels, curtain wall, window wall, living green walls, louvers, or etc. Infill walls should have openings compatible in aesthetics with the adjacent buildings.
 - E. Vehicular access to parking facilities should be designed so as not to obstruct free flow of traffic on adjoining public streets. Vehicular entrances and exits should be no wider than necessary to accommodate anticipated traffic and be located so as to minimize turning movement conflicts as vehicles enter and exit the structure.
 - F. Pedestrian access points to the parking facility should be located to avoid pedestrian/vehicle conflicts.



Parking structures with individual windows and complementary materials (left). Parking structure with a shopfront frontage type and outdoor dining (right).

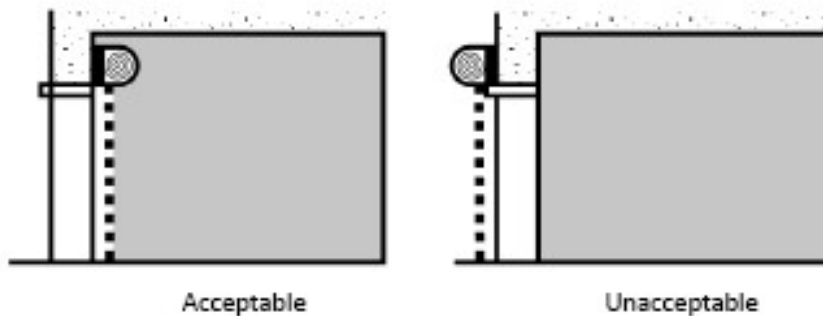


Source: GSky Plant Systems, Inc. (left). Source: Moore Ruble Yudell Architects (right).

4.7 Façade Enhancements and Activities

1. Awnings or canopies over public walkways for storefronts and businesses are encouraged. The size, scale and color of the awnings should be designed to be compatible with the rest of the building; awnings should not be the predominant element of the façade, but should accent the top edge of ground floor windows and door frames. Awnings should not cover the storefront piers or pilasters and should be divided into sections to reflect the major vertical divisions of the façade.

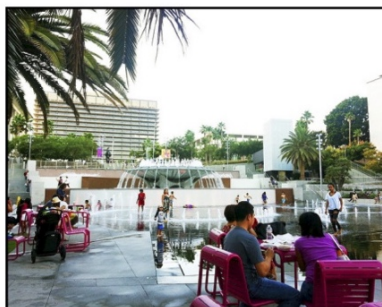
2. The use of solid roll down security grates is prohibited in any location directly visible to the public, including for primary building entries. Alternatives to solid roll down grates include interior roll down grates, architectural features such as the use of wing walls or landscaping, or other security devices that can be completely concealed during regular business hours.



3. Patio Dining on private property is encouraged. Patios may face the sidewalk, or face interior courtyards and open spaces.

4. Sidewalk Dining on public right-of-way is permitted immediately in front of full service or take-out restaurants on streets with a 10' minimum sidewalk. The following additional standards apply:

- A. Display of an outdoor dining menu is permitted on a single-sided framed menu attached to a moveable barrier within the sidewalk or patio dining area. The size of the frame shall not exceed three square feet. Freestanding pedestal menus or A-frame displays are also permitted so long as they do not block pedestrian movement, are moved indoors when the restaurant is closed, and are no greater than 4 square feet per side and no taller than 3' high.



Patio dining facing sidewalk (left) and facing interior open space (center), sidewalk dining (right).

- B. Approval of Encroachment Permit is required from the Public Works Department.
- C. Along the Compton Walk, the outdoor dining area is restricted to the “spill-out zone” as defined in Section 5.3. Along other streets, the outdoor dining area must maintain at least six feet of unobstructed sidewalk.

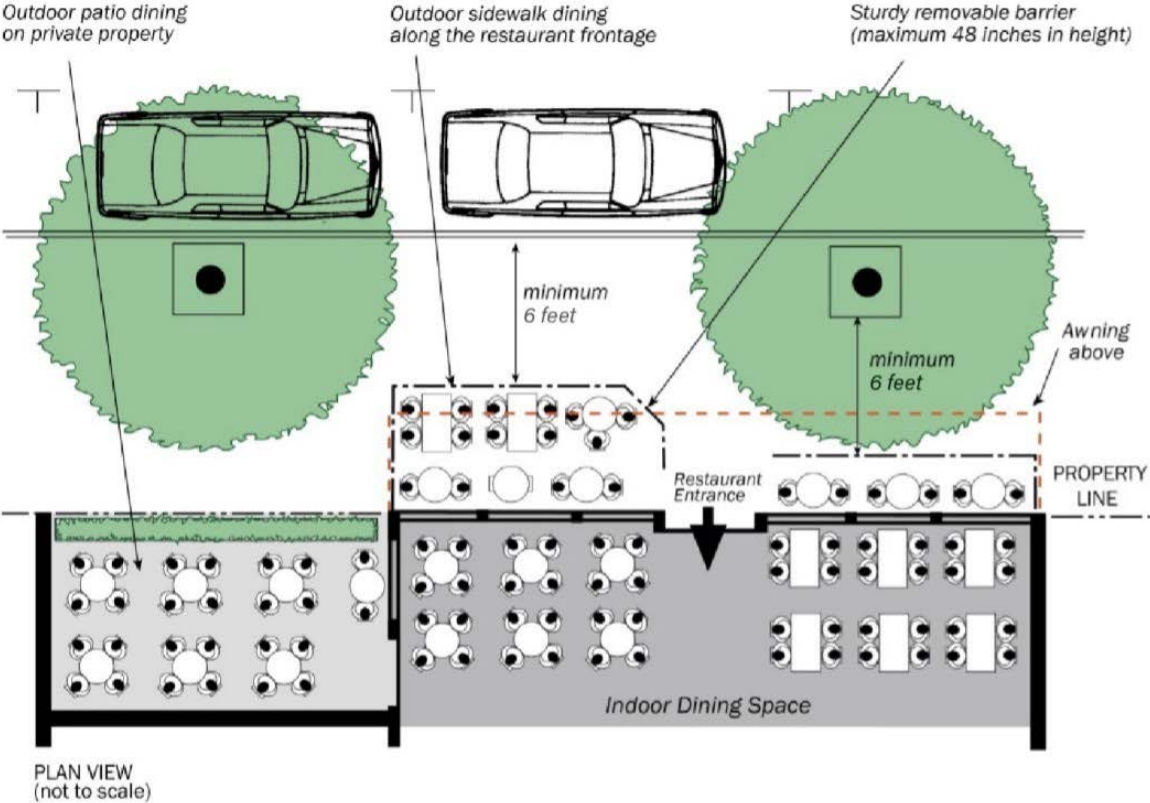


Figure 4-1. Outdoor dining standards for areas other than the Compton Walk.

4.8 Residential Open Space and Amenities

1. Common open space areas shall be convenient to the residential portion of a mixed-use development, separate and secure from the non-residential component, and clearly marked for resident use only.
2. Common open space areas shall contain amenities appropriate to the project's size; e.g., a barbeque area for smaller projects and pools, recreation buildings, or rooms for larger projects. Children's play areas shall be sited to be visible from residential units and not visible from the public right-of-way.
3. Common amenities should be sufficient for and appropriate to the project's target population.
4. Accessible rooftop open space may be used as common useable open space by the residents. It may also be used for private useable open space, when the space is directly accessible from the unit(s).

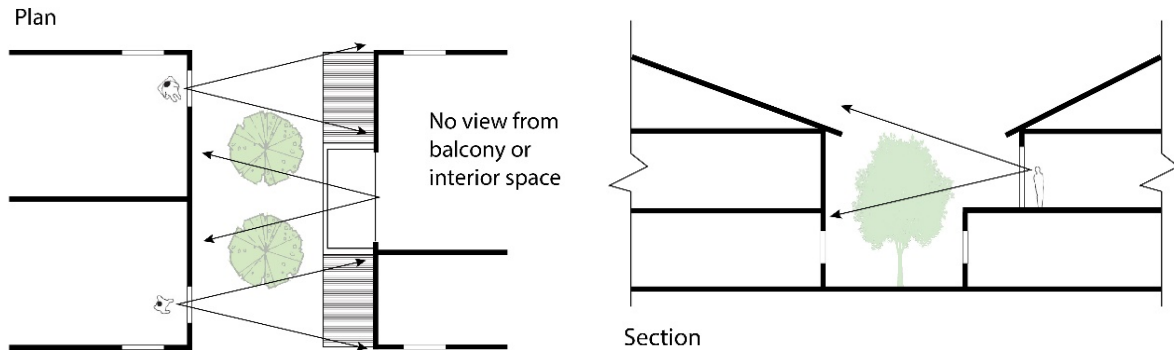


Appropriately scaled and usable open space (left). Child play spaces protected from streets and non-residential uses (right).

4.9 Residential Privacy

1. Given the presence of noisy roads and rail lines in the planning area, residential units shall be constructed and designed to reduce noise. Proper design may include, but shall not be limited to, building orientation, double windows, wall and ceiling insulation, and orientation of vents. Common walls between residential and non-residential uses shall be constructed to minimize the transmission of noise and vibration.
2. Residential uses shall be buffered from adjacent industrial uses and off-site parking by a wall and vegetation.
3. Buildings shall be oriented to promote privacy to the greatest extent possible. Windows of non-residential buildings in a mixed-use development should be oriented to avoid a direct line of sight into adjacent residential buildings or property. In mixed-use developments, residential windows should face away from loading areas and docks. To the extent residential windows face the windows of an adjacent unit, the windows should be offset to maximize privacy. Windows, balconies, or similar openings should be oriented to minimize direct line-of-sight into adjacent units within the development, even if they are one story below or above, or they should be screened through trees. In addition, units above the first story should be designed so that they do not look directly onto private patios or backyards of adjoining residential property or units.

4. Where a mixed-use project abuts a residentially zoned property, a minimum of three feet of the required setbacks adjacent to the residential use shall be devoted entirely to landscaping, with evergreen trees or an evergreen trellis structure at least six feet in height (exclusive of any planter area curb).



4.10 Green and Active Roofs

1. The use of green and active roofs on structures is highly encouraged.
2. The sustainable values of green roofs include stormwater collection, building insulation and urban heat island reduction. Extensive green roofs can be used as planting areas, while intensive roofs can be used by owners and as domestic gardens, small-scale food production, and for general recreation. Stormwater drainage from roofs and the surrounding sites can be captured and harvested for re-use in landscaped areas through low-flow drainage systems.
3. Photovoltaic panels can be installed on green roofs to generate renewable energy while providing shade for rooftop recreation. They can also be used on top of parking garages to generate energy and shade vehicles.
4. Active roofs provide common open space for residential, office and commercial uses. Examples of active roofs include recreational, lounging, and social spaces for multifamily residential uses; residential rooftop pools and rooftop bars. The edges of roof decks that are adjacent to lower off-site residential uses should be located 6 feet back from the edge of the building in order to preserve privacy.
5. Roof patios used for bars or restaurants should have separate access systems than other building uses. Walls or other screening should be provided to inhibit noise from mechanical equipment from affecting on- or off-site residential uses.



Vegetated green roofs (left) and solar parking garage roofs (right).



Rooftop bar with planters separating habitable space from the edge.

4.11 Walls and Fences

1. Exterior solid masonry walls shall be faced to complement the surrounding architecture.
2. Masonry walls of a uniform material shall vary in pattern, texture, and color, and employ elements such as columns, pilasters, capstones, banding, projections, or cornices to interrupt the monotony of continuous lengths of wall.
3. Fences shall be constructed of composite vinyl or wrought iron and include stone or brick piers. Fence colors design shall blend into the landscape.
4. Fencing and walls should be kept as low as possible while performing their screening and security functions. The maximum height of fencing or walls is 4 feet, except for interior property

lines, where it is 6 feet.

4.12 Exterior Lighting

1. Every project should have adequate lighting to provide for security and visibility, particularly along walkways and driveways, entrances to parking areas, and open space areas. The design of light fixtures and their structural support shall be architecturally compatible with the main buildings on-site.
2. Site, parking lot and building security lighting should not impact surrounding or neighboring properties. The type and location of such lighting shall preclude direct glare onto adjoining property, streets, or skyward. All lighting fixtures must be shielded to confine light spread on-site.
3. In mixed use projects, all site, parking lot and building security lighting should be appropriately shielded so as not to spill over into or otherwise adversely impact the residential portion of a mixed use development. Lighting on walking and driving surfaces shall be one foot candle minimum.
4. Accent lighting should be used to draw the eye to important features, complement architectural details, and enhance the enjoyment of the nighttime environment.

4.13 Outdoor Service, Storage, and Loading Areas

1. Outdoor storage and service areas (including, but not limited to, service entrances, loading docks and bays, outdoor storage of commercial vehicles and refuse and recyclable collection facilities) shall be clearly defined and designated for convenient access. They shall not conflict with vehicular access, on-site parking facilities, pedestrian walkways, or customer entrances. Trash or recyclable areas shall not be visible to the public.
2. All outdoor storage and service areas for commercial uses shall be located so as to be convenient to the commercial users and where associated odors and noise will not adversely impact the residential units in a mixed use project, or adjacent residential uses.
3. Outdoor storage and service areas shall not be visible from public right-of-ways and shall be fully concealed or screened from on-site and off-site public view with a combination of building features, decorative walls, and landscaping consistent with the architectural style of the building.



Outdoor service area accessed from shared alley and concealed from view (left) and downward-directed lighting (right).

4.14 Compton Walk Placemaking

The following principles and guidelines are intended to be used by developers and the City to supplement the Compton Walk base streetscape conditions described in Section 5.3 with a yet undefined range of new programming and placemaking interventions intended to pour life into The Walk experience and provide a personalized approach reflective of Compton. These guidelines are envisioned to spark the need to undertake additional projects addressing a comprehensive look at placemaking and experiential development on The Walk. These guidelines are also intended to be used as part of a review/approval process to incorporate more localized, one-off additions to the streetscapes by adjacent businesses and property owners. The magic of the Compton Walk will come with a range of programming and elements, some ephemeral and changing, to promote discovery and exploration.

Streetscape Experiences Customized by Compton

Building on the base 'Score Sheet' streetscape, the Compton community will be encouraged to compose its own melody of artistic, historical, and interactive elements which will be the focus of a journey through cultural Compton. The process of developing the placemaking will need to be highly inclusive and consider a range of perspectives, weaving different elements and layers together to create a cohesive song of experiences. A strong community engagement process is encouraged to ensure all voices are heard and registered in composing a vision for The Walk.

Streetscape Programming and Placemaking Features

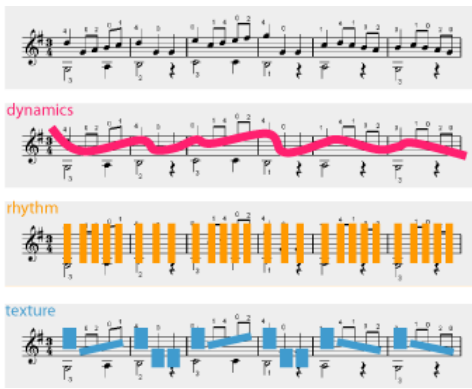
A composition of streetscape programming, art, placemaking, and other ideas will be important layers for Compton Walk to be able to tell its own story and unify an overall walk experience. These layers may include a regular rhythm of elements to hold the streetscape together and other unique, localized placemaking moments to promote further exploration and discovery.

Goals for programming and the development of placemaking along The Walk:

- Develop a range of elements and placemaking that cater to the needs of abutting businesses, the general resident community, the historic community, the creative community, and outside visitors with the goal to serve as broad of a range of user types as possible and in turn increase vibrancy.
- Provide a variety of activities for different types of users, activities, and times of day, also to increase vibrancy.
- Balance elements made to pay tribute to the past with creative, forward-thinking elements and messaging for the future.
- Educate people about the lesser-known stories of Compton's past, while also responding to the general public's love for famous members of the community.
- Provide a range of element types, including vertical elements, multi-media, and lighting to provide visual strong interest.



A range of program types encourage to create vibrancy. A regularly spaced rhythm of program elements to unify the streetscapes (left), one-off placemaking interventions and art (right)



This visualization shows how different combinations of programming (notes) can create very different experiences (songs) along the Compton Walk streetscape and is encouraged.

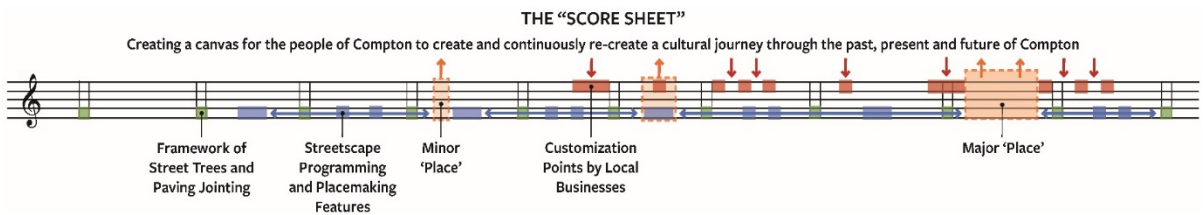


Figure 4-2. Streetscape ‘Score Sheet’ Programming Concept.

4.15 Compton Walk Private Development Interfacing

The following principles and guidelines are intended to be used by developers and the City to better connect adjacent private development to The Walk in a way that feels more seamless, purposeful, and intended with the base streetscape conditions as described in section 5.3. Connection points to adjacent development will provide important beacons along The Walk and gateways to businesses that are activating the streetscape experience. These guidelines are intended to be used as part of a review/approval process for developer/business owner proposals.

Minor 'Places'

This guideline will allow for and encourage local businesses to utilize portions of The Compton Walk streetscape to better engage with the overall walkway experience and promote a clearer connection and gateway to those businesses. Outdoor dining operations, unique business paving and patterning, and other business improvements will be allowed to push out into the edges the streetscape, identified as Streetscape Zone C and subject to the standards presented further in **Section 5.3**.

A series of these smaller enhanced streetscape spaces are envisioned to dot the Compton Walk and act as 'breadcrumbs' for encouraging further exploration and discovery throughout the downtown. Minor 'Places' will be distinguished from the rest of the streetscape with artistic or special paving elements related to the adjacent business operations. Minor 'Places' would be located at key streetscape nodes or building entries to mark adjacent destinations and access points. Several suggested locations have been identified in **Figure 2-10** in red with provisions in gray for others to be implemented over time.



Source: The Apollo Apartments



Major 'Places'

The Compton Walk will provide important connections to the downtown's key civic spaces and parks, which will be experienced as Major 'Places' as part of a comprehensive walk experience. Large, iconic programmatic elements, events, art celebrating Compton greats, and vibrant civic spaces will be featured at these locations to help focus the energy of the downtown at its most important centers. Some of the downtown locations identified for Major 'Places' include, but are not limited to:

- MLK Memorial Plaza (Civic Center) – see Section 2.4
- Metro Plaza (adjacent to Compton Station) – see Section 2.5
- Corner of Compton Blvd. and Tamarind Ave. – see Section 2.5
- Alameda East Greenway Gateway – see Section 2.9
- Willow Plaza – see Section 2.6



Chapter 5: Mobility, Parking and Street Standards

5.1 Mobility Goals and Strategy

The Compton Station Specific Plan will create an environment that facilitates and prioritizes use of public transit, walking, and biking as means to get into and around the center of Compton. These modes of transportation are space efficient and enable more people to be brought into the downtown area to engage with businesses, culture, and the urban environment in general. Less land is dedicated to roadway space and parking lots, and instead it is repurposed for productive uses.

This chapter lays out physical modifications to streets, a parking strategy and mobility-focused regulations on private development which implement the following Specific Plan key actions described in Chapter 1:

Goal #2: A safe and well-maintained Downtown where people enjoy public space.

Action 2.6: Design streets that are intended for pedestrian activity and low speeds.

Action 2.7: Address the safety of streets and intersections, including the streets located along the Metro A Line and Alameda rail corridors.

Goal #4: A station area which promotes public health and sustainability, and provides calm and attractive networks for walking and biking.

Action 4.1: Improve first/last mile access to the Metro A Line (Blue) Compton Station with additional infrastructure and programs that support walking, biking, and rolling.

Action 4.2: Create a diversity of streets supportive of different modes of transportation, and develop facilities for those modes of transportation along the appropriate streets.

Action 4.3: Reconfigure Willowbrook Avenue to reduce safety concerns, improve wayfinding and add low-stress bicycle facilities.

Action 4.4: Develop a greenway on Alameda Street East for recreational/exercise and transportation uses.

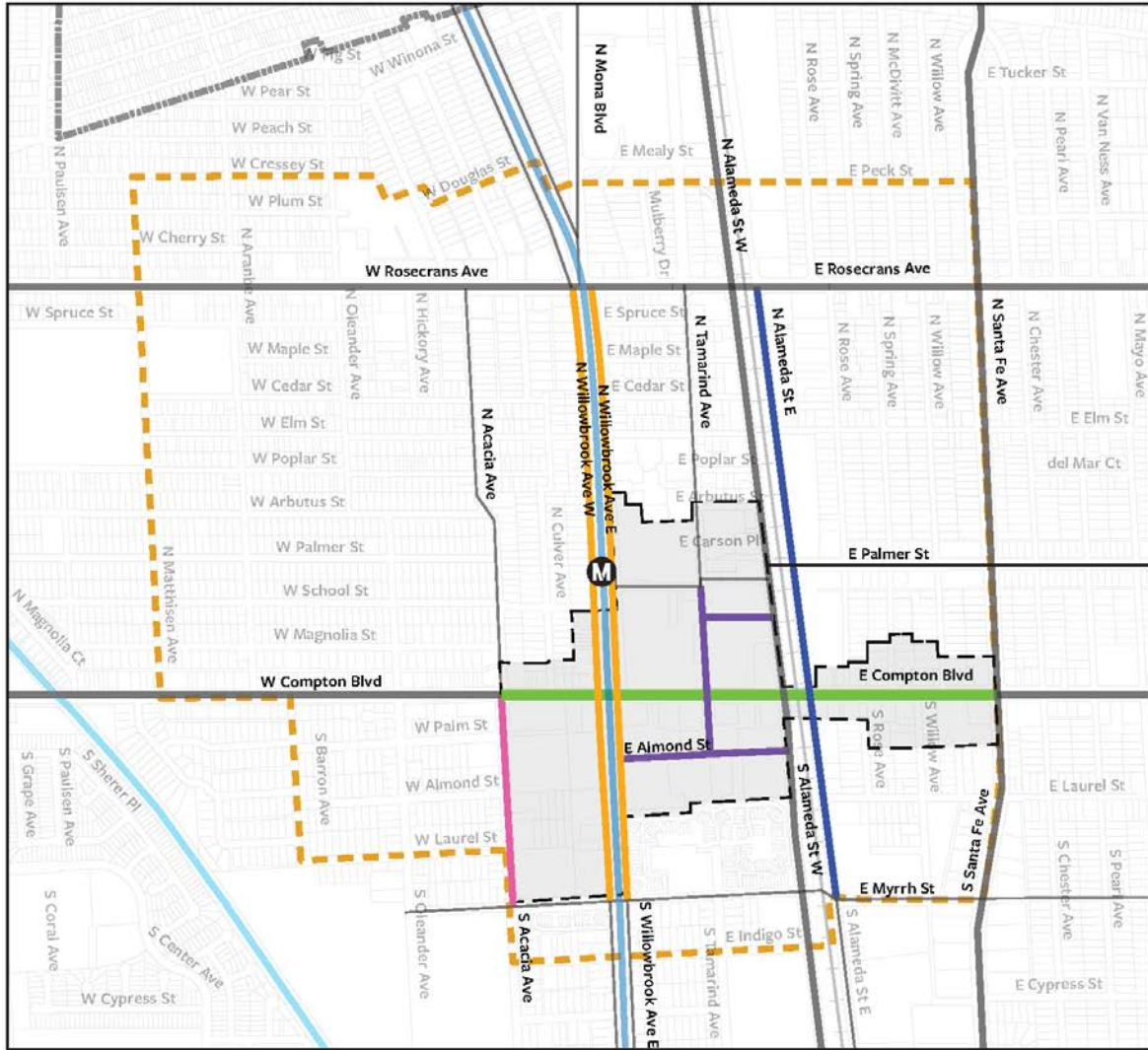
Action 4.5: Reduce minimum parking requirements to encourage residents to travel less by car.

Action 4.6: Use existing and new parking structures to create a shared parking district where residents, employees and visitors can park once and circulate around Downtown on foot.

5.2 Street Network

Figure 5-1 shows the street network for the Specific Plan area, with particular attention to the corridors which this plan proposes modifying. Each of these corridors is described in detail in the sections that follow: Compton Walk/Compton Boulevard Narrowing (Section 5.3), Willowbrook Avenue Couplet (Section 5.4), Alameda East Greenway (Section 5.5), Acacia Avenue Restriping (Section 5.6), and Commercial Core New Streets Network (Section 5.7).

Each of these corridors is proposed to be modified to create a more walkable, bikeable, and transit-oriented station area; however, these corridor modifications also occur within a framework of key access roadways which are the focus for travel into and through the Specific Plan area. As streets designated by the City of Compton General Plan as Major Highways, Rosecrans Avenue, Alameda Street, and Santa Fe Avenue are in particular critical to accommodating vehicular traffic in the Specific Plan area.



Downtown Street Network and Modified Corridors

- City of Compton
- Specific Plan Area
- Metro A Line
- Alameda Corridor
- Compton Creek
- Compton Walk/Compton Blvd Narrowing
- Acacia Ave Restriping
- Alameda East Greenway
- Willowbrook Ave Couplet
- New Commercial Core Streets
- Key Access Roadways (Major & Minor Highway)
- Key Access Roadways (Collector and Local)

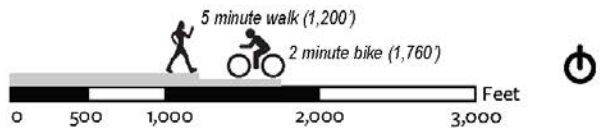


Figure 5-1. Downtown Street Network and Modified Corridors.

5.3 Compton Boulevard

Compton Boulevard between Acacia Avenue (west) and Santa Fe Avenue (east) will be restored as the centerpiece of Downtown Compton and will feature a new Compton Walk experience, further described in Section 2.7 and Sections 4.14 and 4.15. As a true downtown destination, Compton Boulevard will be a place for visitors to slow down, park, walk, and explore. Corridor improvements will deprioritize regional pass-through vehicular traffic and give an increased priority to localized pedestrian walkability and active transportation.

Sidewalks will be widened on both sides of the streets in exchange for a narrowing of the Compton Boulevard roadbed. Class IV protected bike lanes will be provided to encourage cyclists, skateboarders, and other rolling mode users to access Downtown Compton. On-street parking will remain and be interspersed with intermittent loading zones for enhanced curb-side management.

Compton Boulevard's current vehicular flow is near the recommended capacity for a proposed reduced-width street. Although some redistribution of traffic is anticipated, Figure 5-3 demonstrates that parallel streets have sufficient capacity to accommodate the extra vehicular flow.

The redesign of Compton Boulevard may be triggered by new redevelopment projects along the corridor or by City action. As described in Section 2.7, the Compton Walk's base streetscape improvements (described further below) should be constructed by developers together with the project. Customizable historic, art and placemaking elements may be proposed and constructed at the time of project construction by the developer or tenant business, or they may be proposed by the City of Compton as part of a larger district-wide design effort.

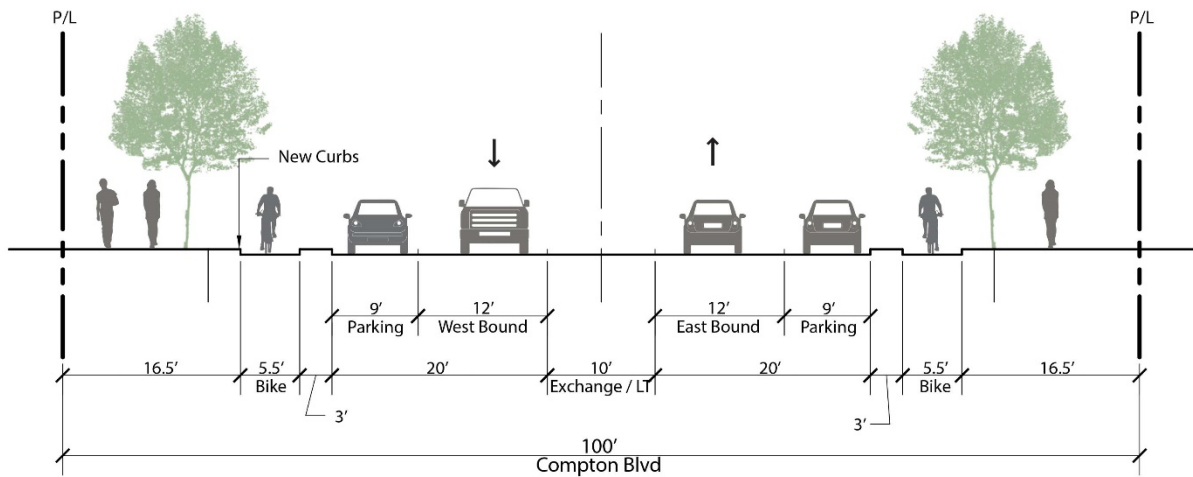
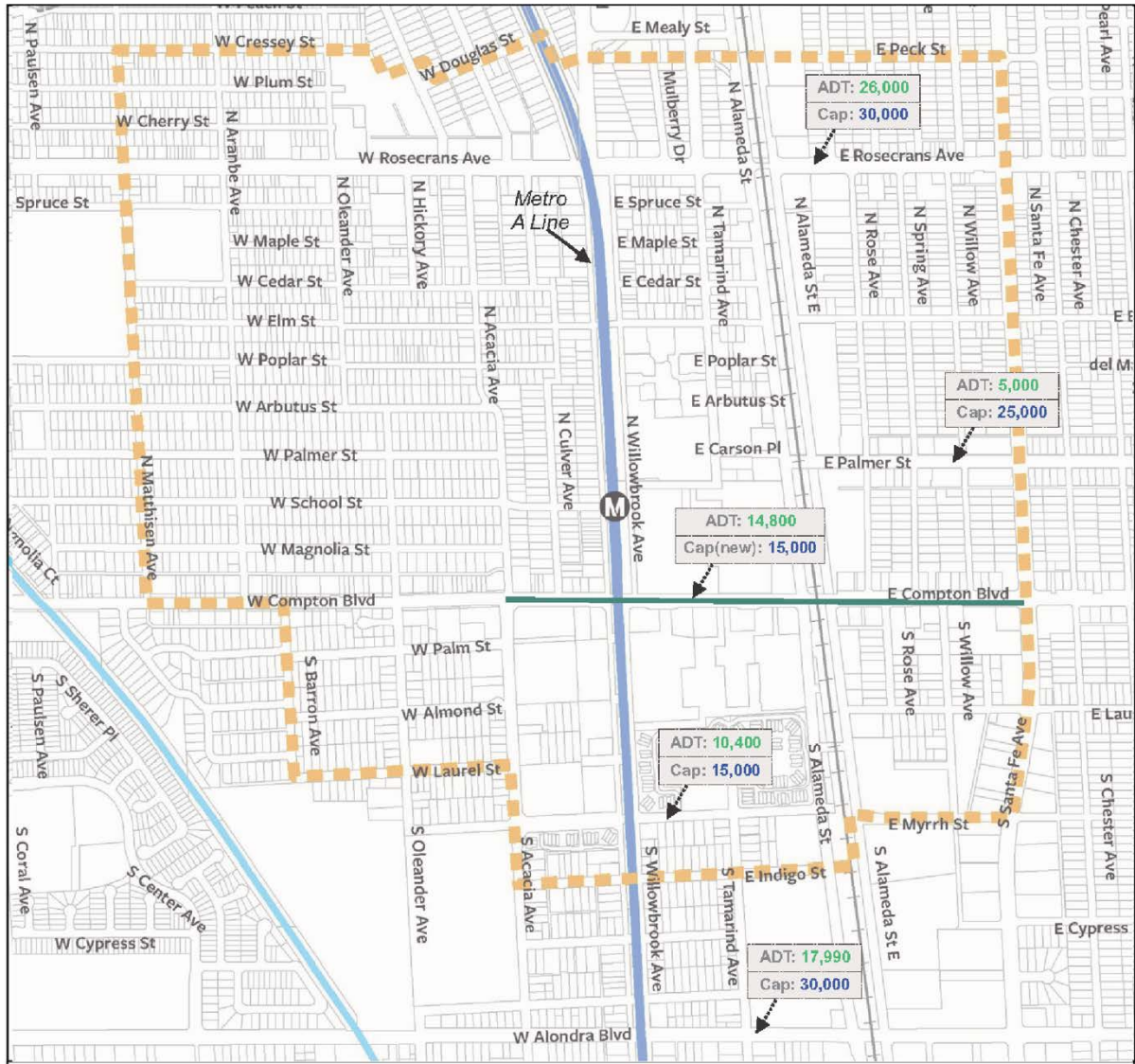


Figure 5-2. Compton Boulevard Typical Cross-Section - Acacia Avenue to Santa Fe Avenue.



Parallel Corridors to Compton Boulevard

- City of Compton
- Specific Plan Area
- Metro A Line
- Alameda Corridor
- Compton Creek
- Compton Walk/Compton Boulevard Narrowing Segment
- ADT Average Daily Trips
- Cap Daily Capacity

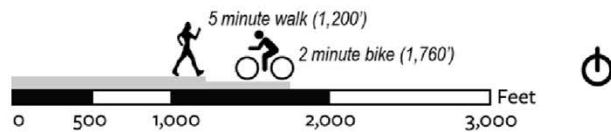


Figure 5-3. Parallel Corridors to Compton Boulevard.

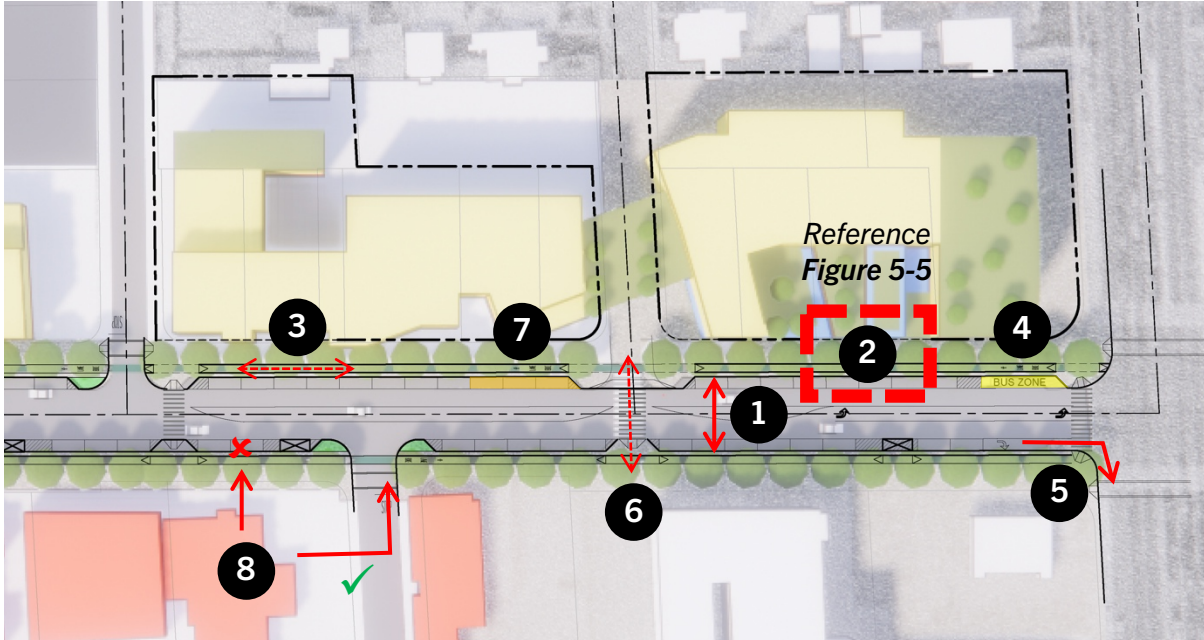


Figure 5-4. Compton Boulevard Typical Corridor Segment and Key Features.

Key Corridor Features:

1. **Reduced roadway paving.** Compton Boulevard roadbed will be shortened from its current 83' width and five (5) vehicular travel lanes to 50' wide and three (3) travel lanes. This roadway reduction will provide space for other mobility uses.
2. **Widened pedestrian sidewalks and The Compton Walk.** The existing 8.5'-wide sidewalks along Compton Blvd will be widened to 16.5'-wide and offer substantially more streetscape space for strolling and other planned programming and activities as a foundation for the Compton Walk experience. Reference Figure 5-5 below for more detailed information on streetscape standards for the Compton Walk.
3. **Separated Class IV bikeways.** Cyclists will be able to ride on a separated Class IV cycle track protected from traffic by parked vehicles and a raised buffer feature. The cycle track could be at roadway level, semi-raised, or at sidewalk level. If the cycle track is not raised to sidewalk level, ramps on the buffer and mid-block crossings of the cycle track should be incorporated to allow handicap access from the sidewalk to parked vehicles.



Class IV separated bikeway (left) and semi-raised bikeway (right). Source (right): NACTO.

4. **Upgraded bus stops with shelters.** Consolidation of stops will be undertaken in coordination with transit providers. Bus stop upgrades shall be provided by adjacent development in keeping with Section 5.15.8.
5. **Dedicated right turn lanes.** Will be added where needed to maintain acceptable operations and Level-of-Service at intersections.
6. **New high-visibility crossings.** To be located at major places, mid-block, and at intersecting streets or walkways as determined by the Public Works Department. No gap larger than six hundred feet should be left between unmarked crosswalks.
7. **Vehicular loading zones.** To be placed intermittently along corridor. A minimum of one loading zone per block should be provided.
8. **Access management.** Curb cuts facing Compton Boulevard to be minimized or eliminated through negotiation with property owners and through redevelopment. Excessive vehicular curb cuts interrupt other multi-modal transit along the corridor and create dangerous conflict points with turning vehicular traffic.

Figure 5-5 illustrates the prescribed Compton Boulevard streetscape zones and associated standards below for how the streetscape areas are to be used.

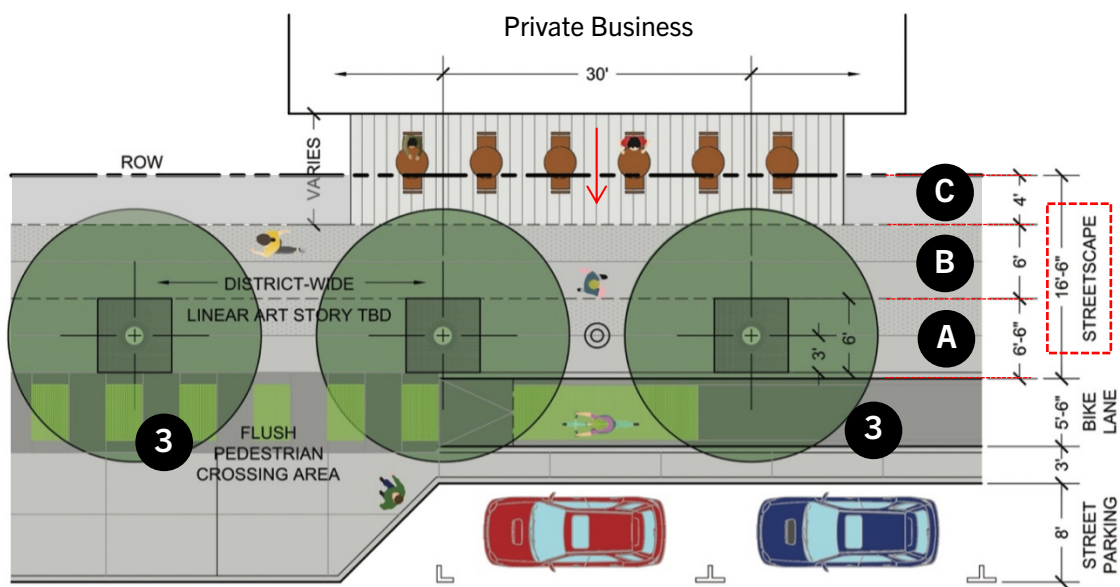


Figure 5-5. Compton Boulevard Streetscape Zones.

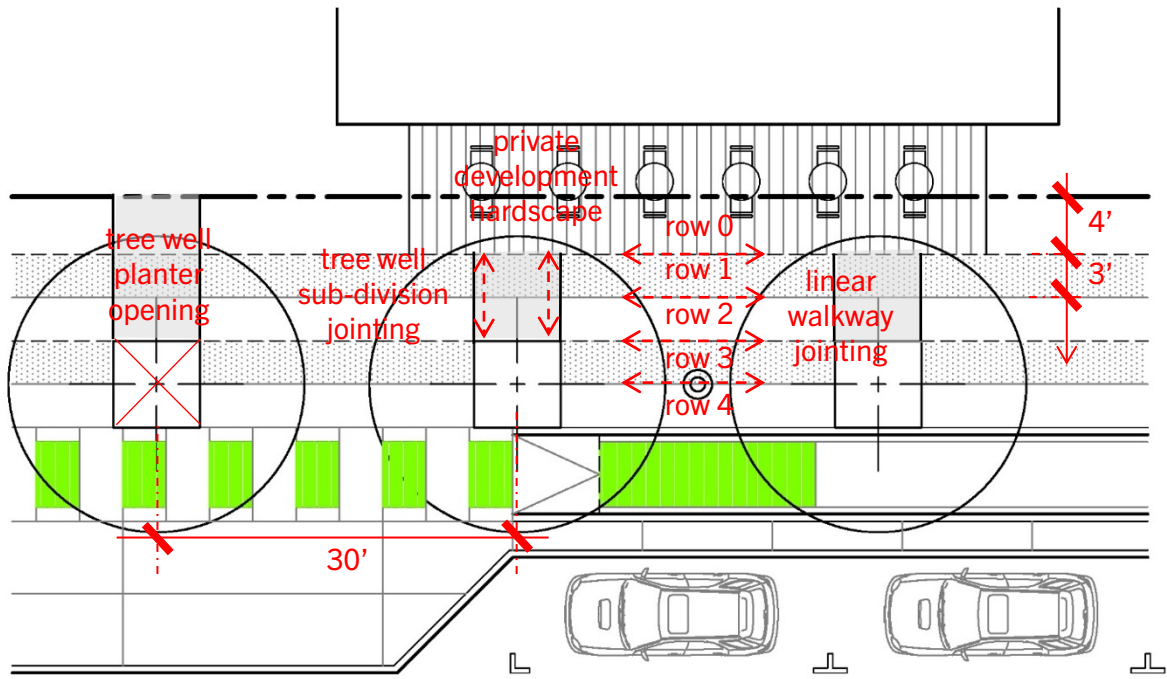


Figure 5-6. Typical 'Score Sheet' Streetscape.

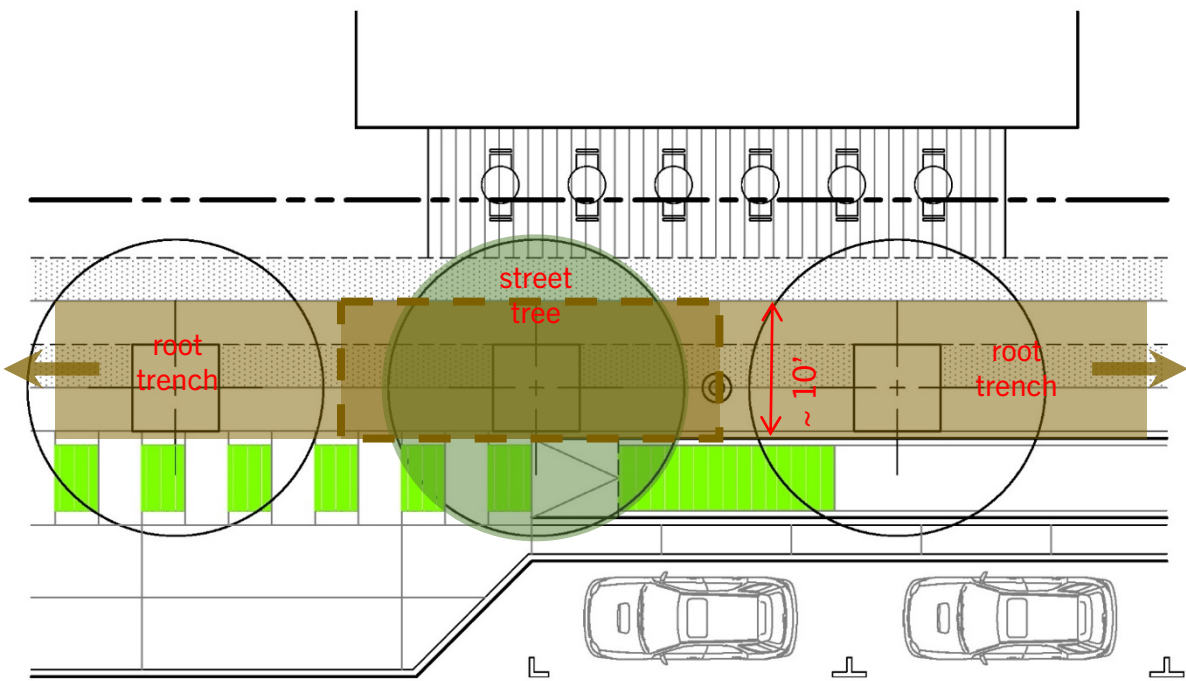


Figure 5-7. Compton Walk Landscape Infrastructure Systems.

Streetscape Zone A

Streetscape zone A denotes the Sidewalk Amenity Zone and will be the place to locate the majority of fixed streetscape elements, street trees, and other roadway infrastructure. This zone shall be a minimum of 6 feet 6 inches wide running along the roadway curb and adjacent to the bike lane. Reference **Figure 5-5**.

Zone A Standards and Required Features:

1. Hardscape Paving (reference **Figure 5-6**).
 - a. Typical 'Score Sheet' paving
 - i. Linear Walkway Jointing
 - ii. Tree Well Sub-Division Jointing
 - b. Tree well planters. 6' x 6' square openings in pavement at each required street tree location.
2. Street Trees (reference **Figure 5-7**).
 - a. London Planetree (*Platanus acerifolia* 'Columbia') Trees shall be planted longitudinally every 30 feet along adjacent property frontage, or starting 30 feet from adjacent property street trees, or as determined per City approval. See supplemental description below regarding the selection of this tree species.
 - b. A subterranean root trench shall be installed laterally along the streetscape which is at least 10 feet wide (it may go under other streetscape zones as well) and provides a minimum of 1,000 cubic feet of planting soil volume for each street tree.
3. Street Furniture
 - a. Tree grates. To be provided at each tree well and street tree location. Tree grate selection encouraged to align with district-wide placemaking and branding guidelines.
 - b. Trash cans should be provided at intersections and every 200 feet.
 - c. Benches or other outdoor seating elements should be provided every 200 feet.
 - d. Bike racks should be provided every 200 feet.
 - e. Pedestrian light poles should be provided every 100 feet.
 - f. Specific street furniture should be selected by the City during a full design process for the Compton Walk. Selection of furniture aligned to the branding objectives of the larger district is encouraged.
4. Utility Infrastructure Elements
 - a. Including but not limited to: roadway lighting poles, fire hydrants, utility vaults, etc.
5. Placemaking, Art, and District Branding (*Optional*)
 - a. TBD – Placemaking, art and branding features are optional and may be proposed by a developer or fronting business. They may also be proposed by the City as part of a district-wide Compton Walk design or arts project. These features will be subject to the guidelines described in Section 4.14 and procedures noted for Public Art in Section 3.10. Their approval will be considered as part of the project

design review permit.

Streetscape Zone B

Streetscape zone B denotes the Sidewalk Pedestrian Zone and will be the area for primary walking and passage activity along the streetscape. This zone shall be 6-foot-wide and shall remain clear of all obstructions to pedestrian access and movement. Reference **Figure 5-5**.

Zone B Standards and Required Features:

1. Hardscape Paving (reference **Figure 5-6**).
 - a. Typical 'Score Sheet' paving.
 - b. Shall be a fully unobstructed paved area.
2. Placemaking, Art, and District Branding (*Optional*)
 - a. TBD - Placemaking, art and branding features are optional and may be proposed by a developer or fronting business. They may also be proposed by the City as part of a full focused Compton Walk design or arts project. These features will be subject to the guidelines described in Section 4.14 and procedures noted for Public Art in Section 3.10. Their approval will be considered as part of the project design review permit.
 - b. In-grade or surface treatment elements only; no vertical elements this zone.

Streetscape Zone C

Streetscape zone C denotes the Business Spill-out and Transition Zone which may contain temporary elements and non-obstructive permanent elements which serve the neighboring developments. This streetscape zone is intended to allow customization of the streetscapes to help feature local businesses and provide clear access and gateway. Guidelines for such customization and placemaking are provided in Section 4.14 and 4.15. Permanent furniture and any type of fencing is not allowed in this zone, although it may be located on private property immediately adjacent. This zone shall be 4-foot-wide running along the adjacent private property right-of-way line. Reference **Figure 5-5**.

Zone C Standards and Required Features:

1. Hardscape Paving (reference **Figure 5-6**).
 - a. Typical 'Score Sheet' paving.
 - b. Private development carry-over and transition paving TBD. Per City review and approval.
2. Street Furniture
 - a. Moveable furniture, including loose tables and chairs from adjacent restaurants and outdoor dining areas; no fixed furniture elements.
 - b. Moveable business advertising and menu signage subject to Figure 3-27 and

Section 4.7.4.A.

- c. Moveable planters subject to Figure 3-24 and City approval of private business standards.
 - d. Public art per City review and approval.
3. Placemaking, Art, and District Branding (*Optional*)
- a. TBD - Placemaking, art and branding features are optional and may be proposed by a developer or fronting business. They may also be proposed by the City as part of a full focused Compton Walk design or arts project. These features will be subject to the guidelines described in Section 4.14 and procedures noted for Public Art in Section 3.10. Their approval will be considered as part of the project design review permit.
 - b. Interface to Major and Minor 'Places', per design review approval.

5.4 Willowbrook Avenue

Willowbrook Avenue is a key street connecting the Specific Plan area to the Metro station. The street is separated by the Metro A Line (Blue) tracks and freight rail tracks into two separate roadways, Willowbrook Avenue West and East. Each roadway is currently configured a two-way street with parking on one side and no bicycle facilities. Willowbrook Avenue West is particularly narrow and does not meet current standard guidance for travel or parking lane width.

The two-way configuration of each side of Willowbrook Avenue is the cause of significant confusion when drivers and pedestrians encounter it, particularly at its intersection with Compton Boulevard. Some common safety issues are illustrated in Figure 5-8 below. Additionally, turns are prohibited in multiple directions, forcing drivers to go around large blocks to reach their destinations.

The planning team developed three conceptual alternatives to solve these issues and sought feedback on them through the virtual workshop, social media, and stakeholder meetings. Alternative A, the Traditional Couplet, converted the street into a one-way pair with southbound traffic on Willowbrook West and northbound traffic on Willowbrook East, and bike lanes on each side. Because traffic flowed in each direction to the right of the tracks, this alternative was the most intuitive. However, it would require the removal and reorientation of the existing MLK, Jr. bus station, the reconstruction of the railroad gate system on all legs of the Compton/Willowbrook intersection, and lengthening of Sheriff's Department paths to Compton Boulevard.

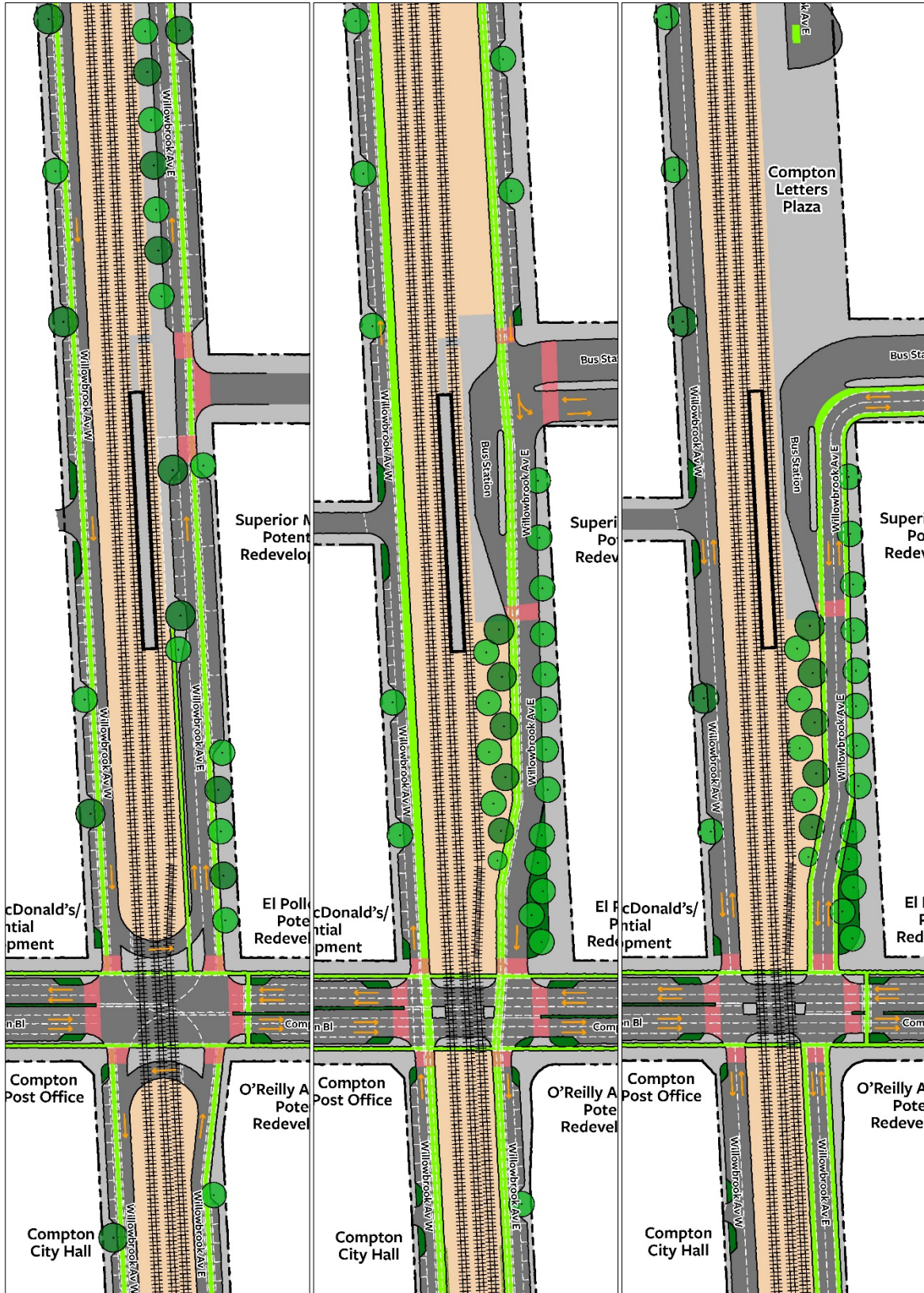
Alternative B, Reverse Couplet, also created a one-way pair, but with northbound traffic on Willowbrook W and southbound traffic on Willowbrook E. While less intuitive than A, this alternative eliminated points of conflict between pedestrians and vehicles at intersections, improving safety. Like Alternative A, it provided space for bike lanes on both sides, and it required less infrastructure investment, as the existing bus station and Metro gate system could be preserved. There may also be new back access for Sheriff's Department vehicles off of S. Acacia.

Alternative C, the Existing Pattern, retained the existing configuration and its advantages and disadvantages. Bike lanes could be added to Willowbrook East, but not to Willowbrook West.

After dialogue with the community and Metro, the City Council in October 2020 agreed with the planning team's recommendation to choose Alternative B as the preferred alternative. All three alternatives are included on the following page, however, as a resource for more detailed design. A fuller description of the preferred alternative is provided after.



Figure 5-8. Common Safety Issues at Willowbrook Avenue intersections.



Alternative A-Traditional Couplet Alternative B-Reverse Couplet Alternative C-Existing Pattern
 Figure 5-9. Willowbrook Avenue Design Alternatives.

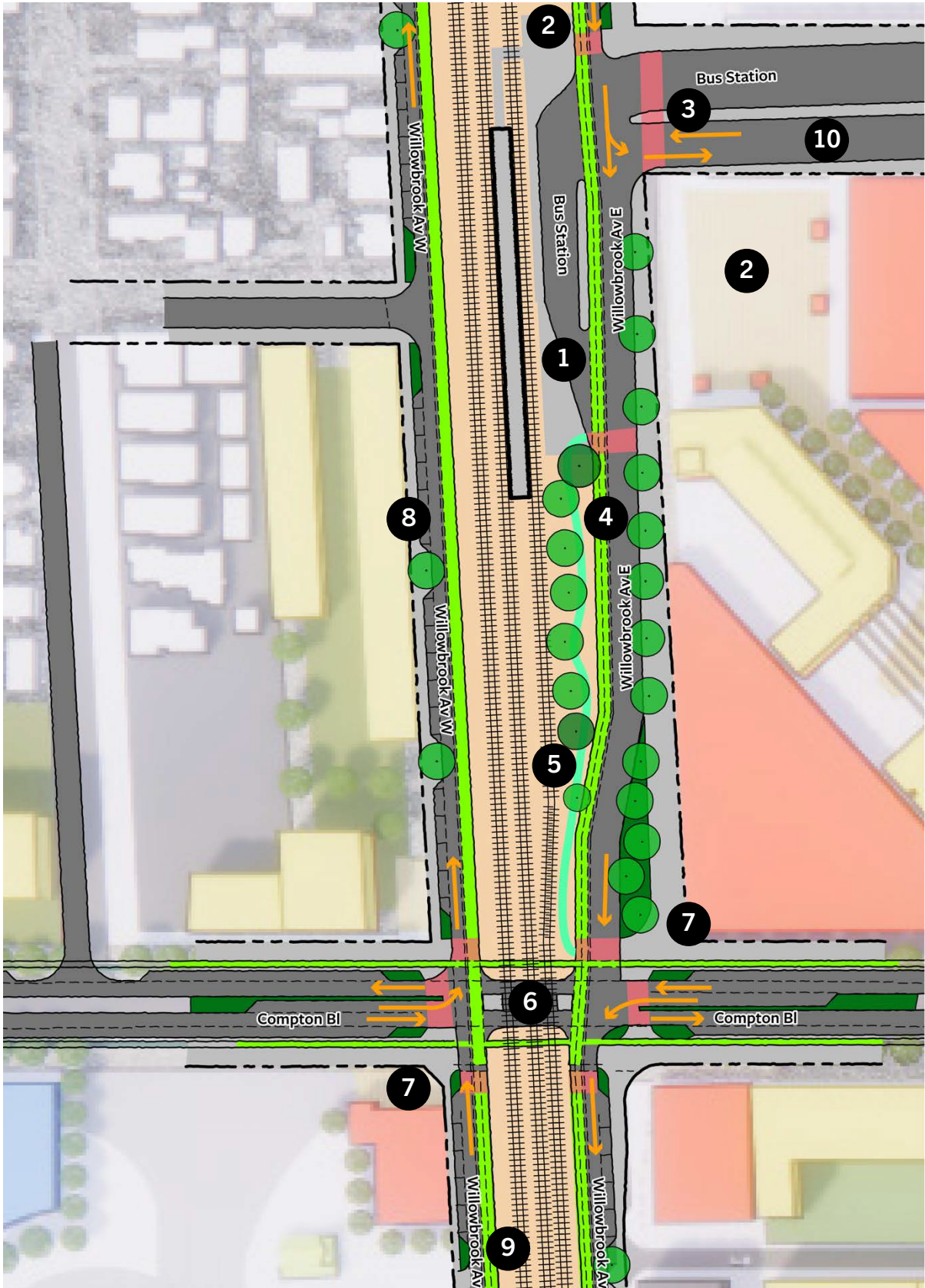


Figure 5-10. Willowbrook Avenue Preferred Alternative (B-Reverse Couplet) Plan and Key Features.

Figures 5-11 through 5-14 below illustrate the typical existing street conditions on Willowbrook Avenue. These cross-sections may vary slightly in different areas based on existing right of way widths and curb to curb widths.

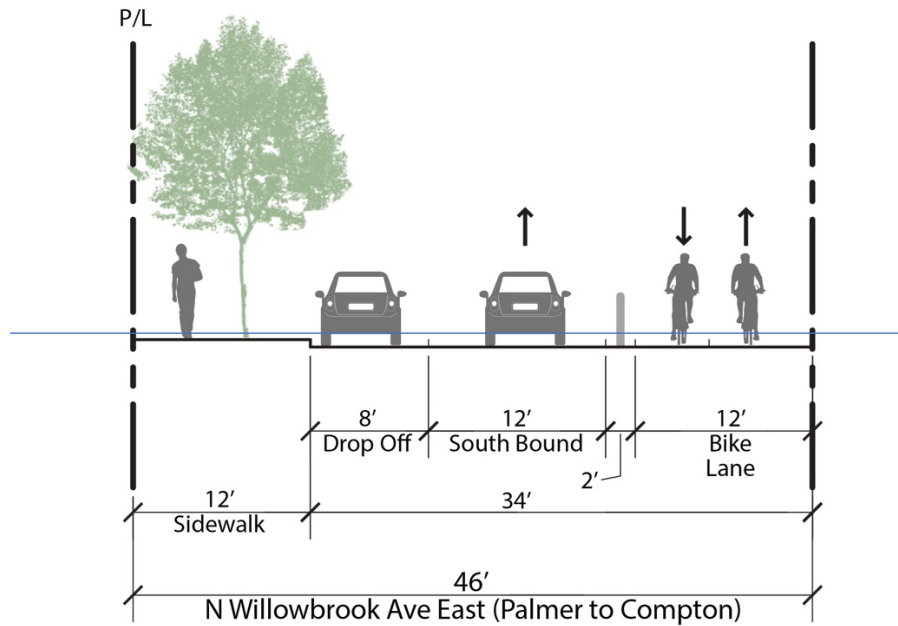


Figure 5-11. North Willowbrook Avenue East (Palmer to Compton) Typical Cross-Section.

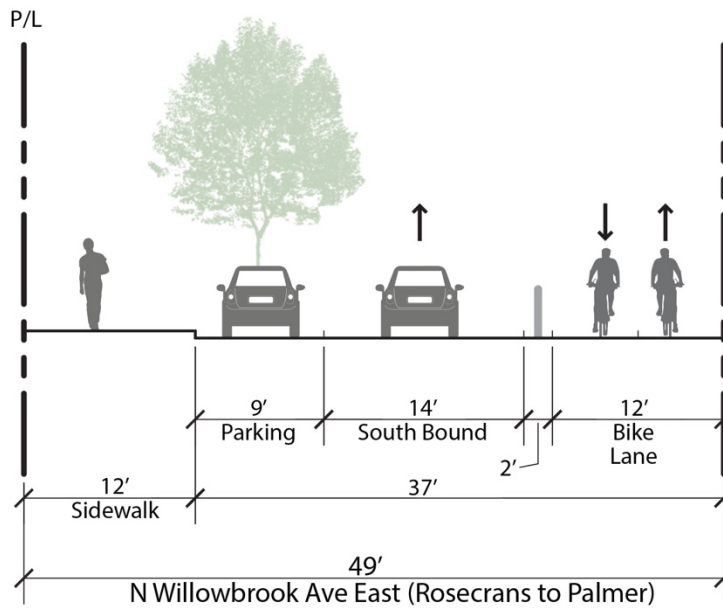


Figure 5-12. North Willowbrook Avenue East (Rosecrans to Palmer) Typical Cross-Section.

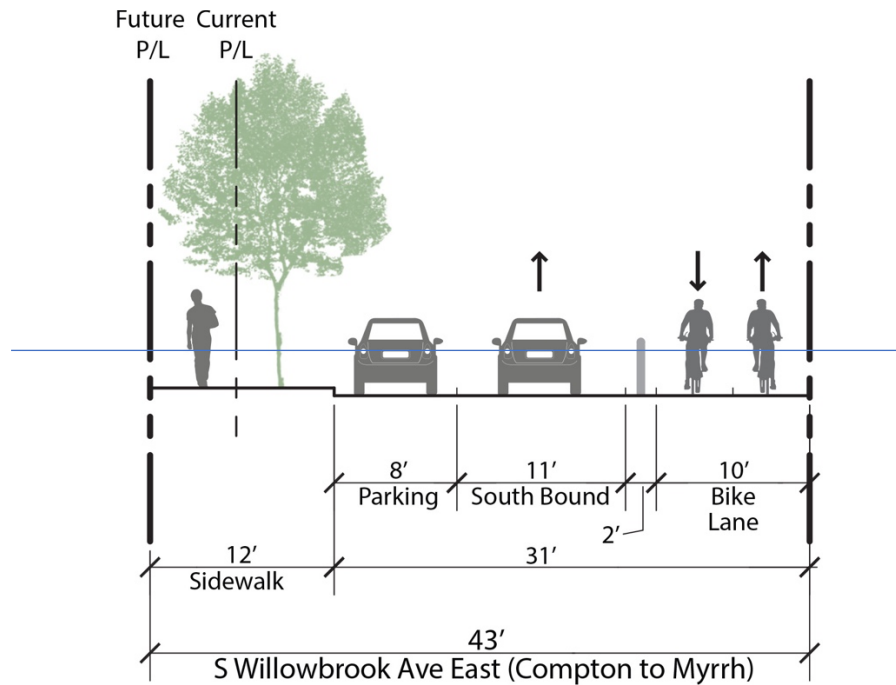


Figure 5-13. South Willowbrook Avenue East (Compton to Myrrh) Typical Cross-Section.

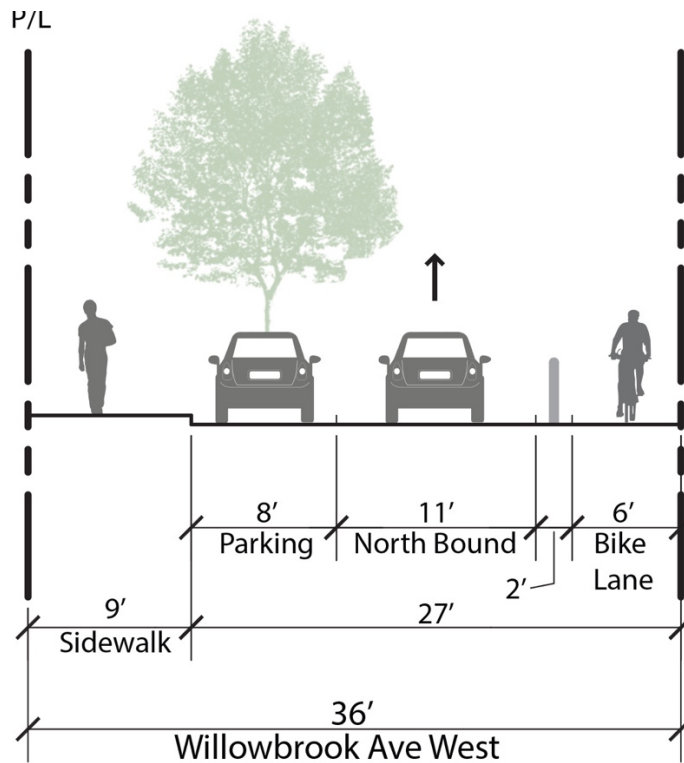


Figure 5-14. Willowbrook Avenue West Typical Cross-Section.

Key Features:

1. Maintain southbound traffic on Willowbrook Avenue East in order to preserve the direction of travel of existing MLK bus station.
2. Replacement of Compton Letters Plaza with a new Metro Plaza on southeast corner of Willowbrook Avenue and Palmer Street. Continuous Willowbrook Avenue will provide direct connection from Rosecrans Avenue to Commercial Core.
3. Raised pedestrian crossings connecting the Metro station to the Metro Plaza.



4. Two-way separated bikeway along the west side of Willowbrook Avenue East providing direct access to the station and to Compton Boulevard. The northbound bikeway could also be accommodated on east side of the street as contraflow bikeway.



Two-way separated bikeway (left) and contraflow bikeway (right, far side). Contraflow bikeway image sourced from the National Association of City Transportation Officials.

5. Increased shade along pedestrian pathway that connects directly to the station.
6. Concurrent left turns from Compton Boulevard onto Willowbrook Avenue.
7. Squared-off corners and bulb-outs to shorten crossing distances, provide double ramps for wheelchair/stroller users, and visually inform drivers that they cannot turn against traffic.
8. Add trees in parking stalls at a regular rhythm to provide shade for pedestrians.
9. Extend bikeways up and down both sides of Willowbrook Avenue. To the extent possible, bidirectional bikeways are preferred in order to serve travel needs of users located along long blocks who do not have easy access to the opposite side of the street. Install regular or, where needed, collapsible bollards to ensure adequate fire truck access while providing safety to riders along the street.

10. Eliminate excess space from Palmer Street to create a shorter crossing and more plaza/buildable area on the south side of the street.

The couplet concept is envisioned between Rosecrans Avenue and Myrrh Street. At either end, the street will transition back to its existing configuration as shown in Figure 5-15.

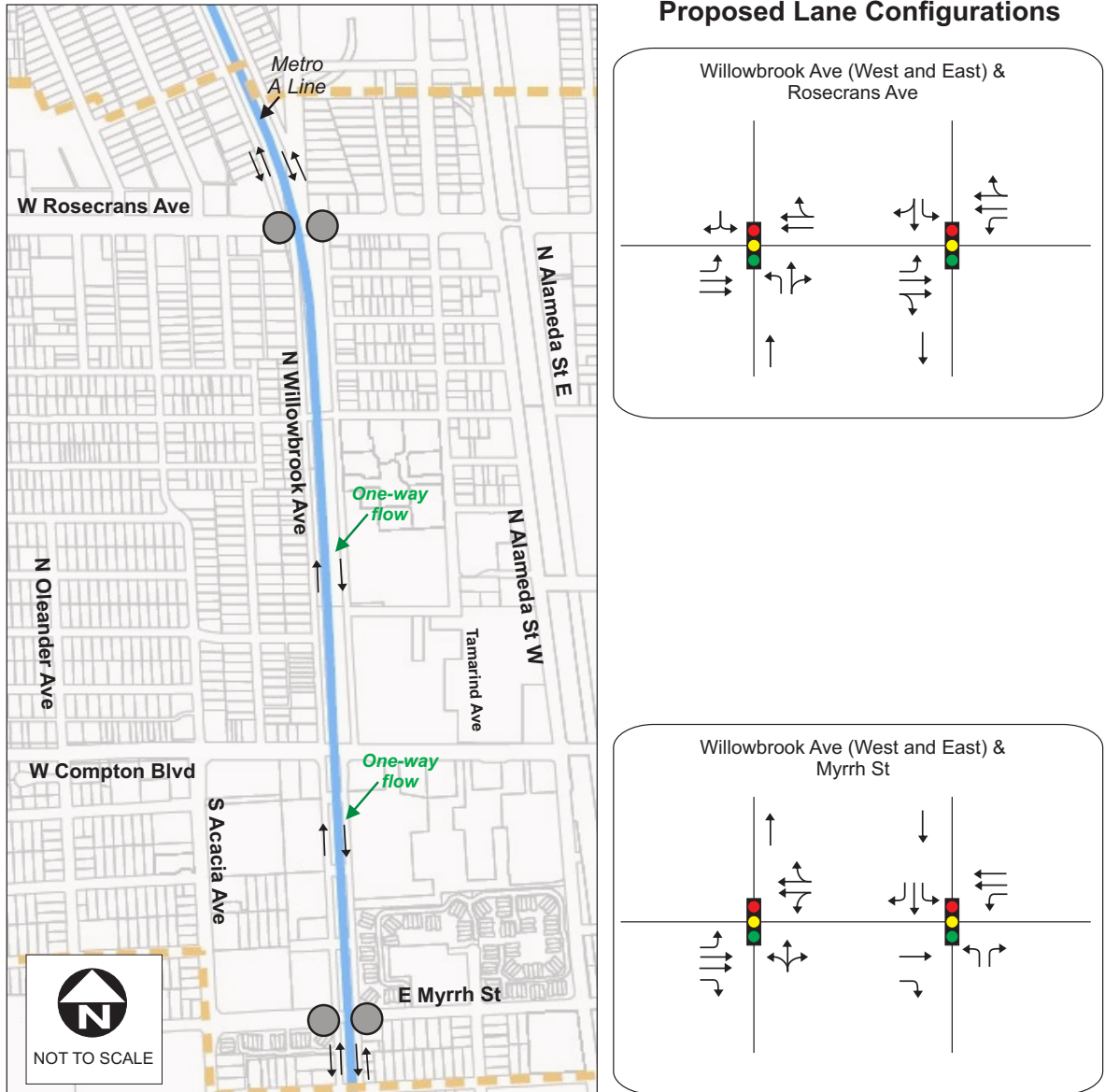


Figure 5-15. Willowbrook Avenue Couplet Extents and End Transitions.

5.5 Alameda East

Alameda Street East between Rosecrans Avenue and Compton Boulevard is currently a lightly-traveled street with traffic volumes less than 30% of capacity. As illustrated in Figure 5-16, the roadway is wide, with 16' travel lanes. Only a few properties draw their access from the street, and these properties mostly have frontages on side streets as well.

The community vision for Alameda East, represented in Figure 5-17, is to close the roadway and convert it to a greenway serving as a recreational and health amenity for the central Compton population. This greenway is envisioned to incorporate a softscape, decomposed granite path, for jogging, as well as a hardscape path for bicycling and other rolling modes. Stormwater treatment and/or infiltration elements could be added, and planting would help provide a buffer to the at-grade and below-grade rail lines just to the west. On its eastern side, up to 10' of the right-of-way could be granted as an easement for greenery, porches, and other frontage treatments for adjoining residential development. This would help development be able to fit in these narrow publicly-owned sites, and help provide eyes on the greenway to ensure safety.

Should access considerations preclude the street from being closed entirely the project may move forward with the reduced greenway alternative, as shown in Figure 5-18. The reduced greenway alternative is the preferred design concept with the portion of the street between Compton Boulevard and Myrrh Avenue.

Figure 5-19 and the text on the following page describe the physical and temporal plan for implementation of the greenway per the preferred plan.

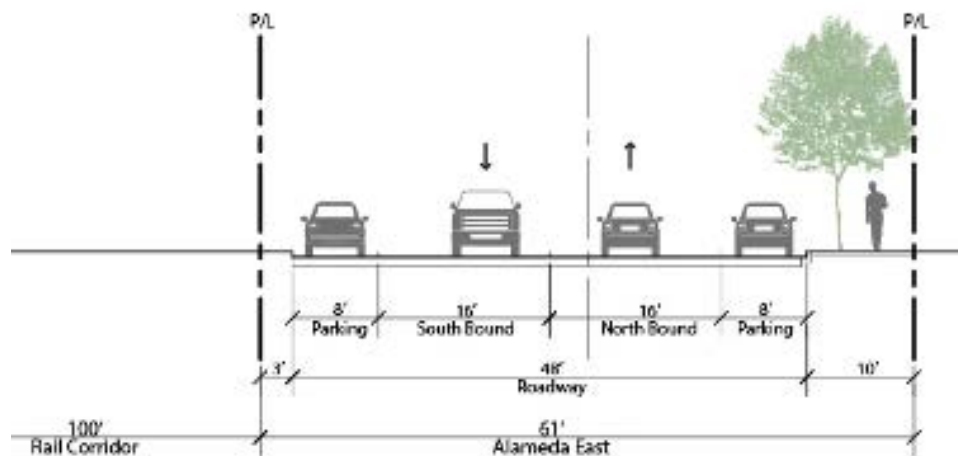


Figure 5-16. Alameda Street East – Existing Cross-Section.

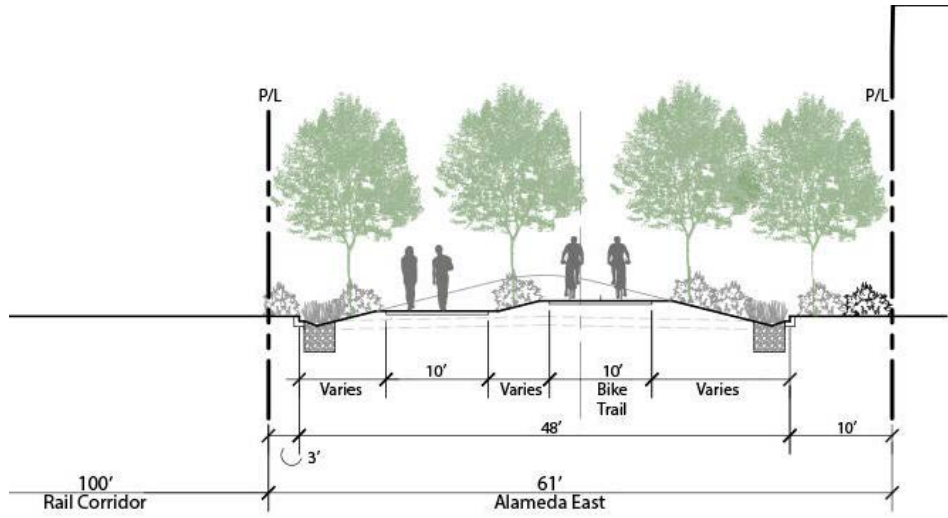


Figure 5-17. Alameda East Greenway Typical Cross-Section (Preferred Alternative).

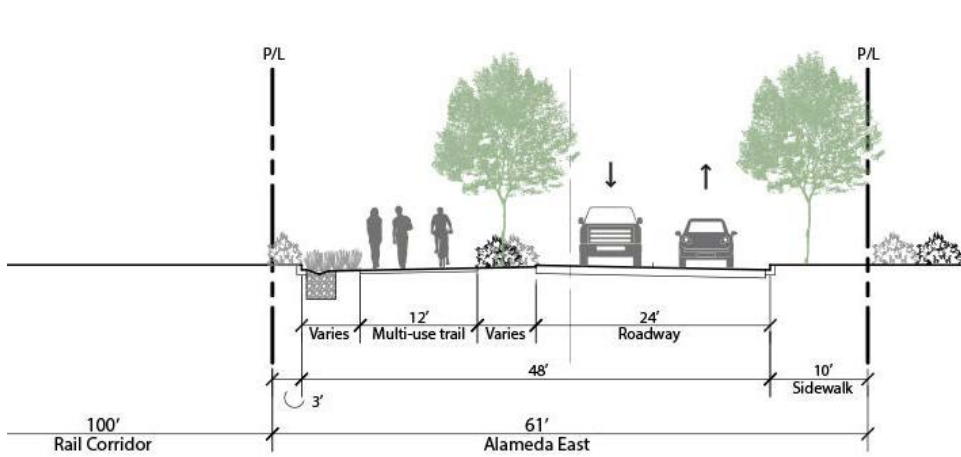


Figure 5-18. Alameda East Greenway Typical Cross-Section (Reduced Greenway Alternative).

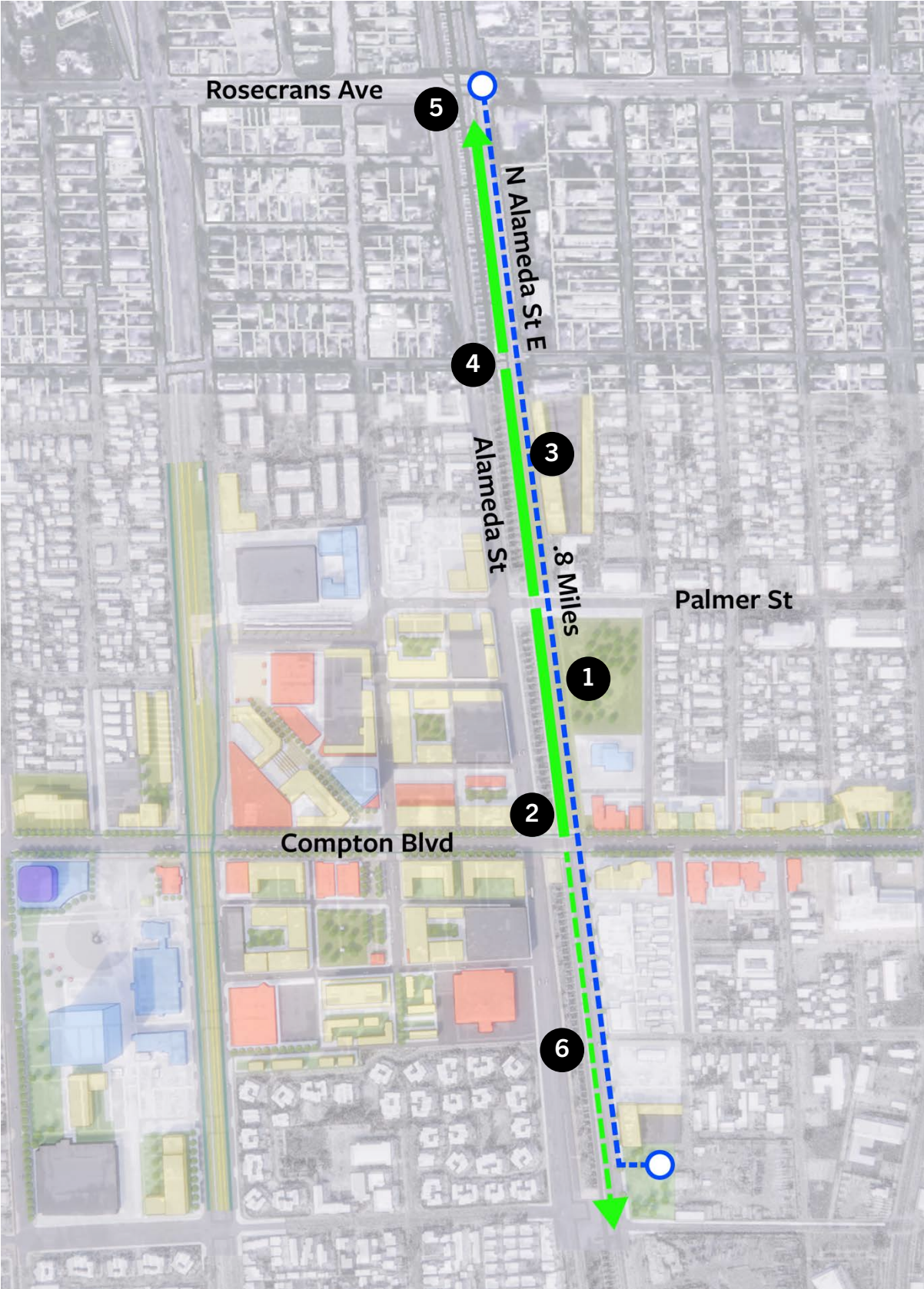


Figure 5-19. Alameda East Greenway Plan and Key Features.

1. Initiate greenway with street closure adjacent to Wilson Park.



Trail and Greenbelt.

2. Work with adjacent business owners to determine appropriate treatment at greenway exit to Compton Boulevard. Consider dining areas and one-way (entry only) travel lane.
3. Expand into adjacent blocks as development permits vehicular access points to be moved to east-west streets.
4. Establish safe street crossings.
5. Open alternate access to Rosecrans Avenue by extending the frontage roads across the Alameda Corridor.



New vehicular circulation patterns enabled by extension of Rosecrans Avenue frontage roads. Bold arrows indicate key movements currently accommodated through Alameda East.

6. Build tree wells and plant trees in parking lane, or consider alternate methods to continue greenway to new garden or recreational space at City Yard.

5.6 Acacia Avenue

Acacia Avenue is a local/collector street which connects Compton High School and the Civic Center with residential neighborhoods. The street is one lane in each direction except in the block between Compton Boulevard and Myrrh Street, where it is two lanes in each direction. Although access to the Civic Center parking garage is from this segment of the street, traffic volume is still well under capacity. Under the Specific Plan, the street will be restriped, reducing vehicular lanes to one in each direction with a center turn lane in order to add safer pedestrian crossings and a Class II bicycle lane in each direction.

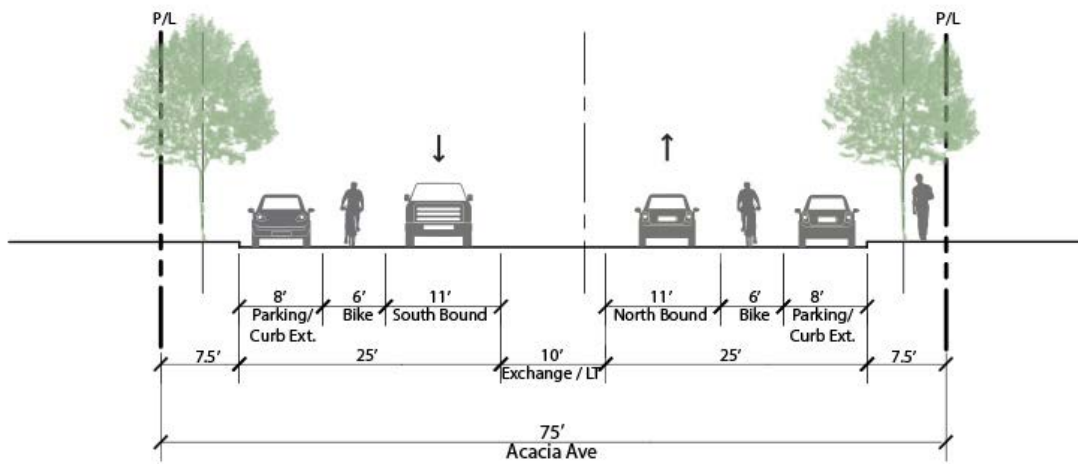


Figure 5-20. Acacia Avenue Typical Cross-Section, Compton Boulevard to Myrrh Avenue.



5.7 Commercial Core New Streets Network

The Commercial Core area currently consists of two superblocks, each containing a 16-17 acre shopping center. As development proceeds in this area, a new network of streets and pedestrian ways will be built to create a walkable pattern in this area. New development shall be conditioned to establish mobility connections and open spaces within the footprint of the proposed development that meet the criteria listed below. If the footprint of the proposed development is less than the full extent of one of the two shopping centers, the applicant shall present a phasing plan demonstrating how the initial phase of the project fits within a larger plan to fulfill these regulations.

Regulations:

1. Tamarind Avenue shall be extended south from its current terminus at Palmer Street to Almond Street.
2. A new east-west street (here denominated Almond Street) shall be established from Willowbrook Avenue to Alameda Street at least 300 feet south of Compton Boulevard.
3. No other public access roadway or curb cut shall be accessed from Compton Boulevard.
4. Within areas B, C, and D as shown on Figure 5-21, a mid-block passthrough shall be provided at least once every 250 feet from Compton Boulevard to the first public access walkway or roadway parallel to the street.
5. No distance between consecutive mid-block passthroughs, shared alleys, or streets shall be longer than 400 feet in areas B, C, or D.
6. A continuous pedestrian connection shall be established between the diagonal and Alameda Street.
7. A public plaza shall be established at the southeast corner of Palmer Street and Willowbrook Avenue, and another public plaza shall be established at the northwest corner of Compton Boulevard and Tamarind Avenue. These public plazas shall be connected with a pedestrian mall.
8. A Central Park shall be established within areas C, D, or E. The Central Park shall be connected to Compton Boulevard with an appropriate walkway.
9. At least one public or private parking facility shall be provided within each area. Each parking facility shall be located in areas which maximize trips on Alameda Street, Palmer Street, and Almond Street, and minimize trips on Compton Boulevard and Willowbrook Avenue.
10. All public parking facilities shall be accessed from an existing or new street.



Figure 5-21. Illustration of Commercial Core Street and Open Space Network Standards.

5.8 New Streets.

New streets shall be created within the Commercial Core area. New streets may be dedicated to the City of Compton or maintained as private streets at the discretion of the Community Development Department Director. In the event that streets remain under private ownership, they must be left open to the public at all times. All new streets within the Commercial Core area should comply with the cross-sectional dimensions listed in Figures 5-22 or 5-23. The use of permeable pavers is encouraged in parking areas.

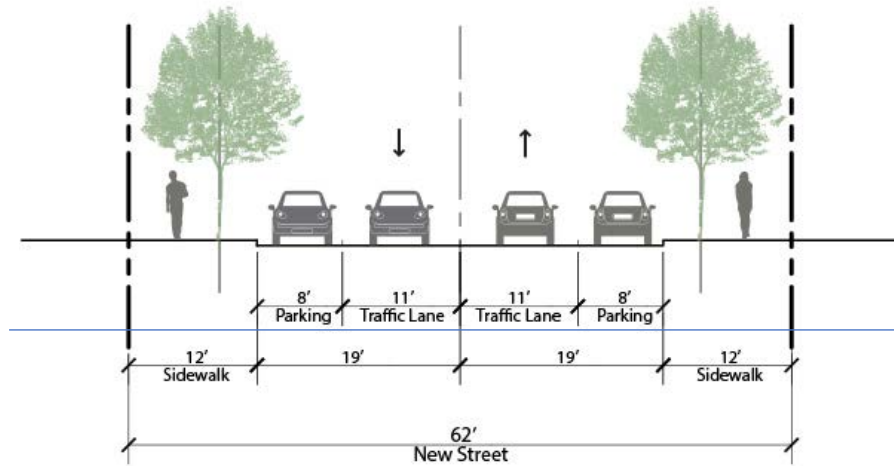


Figure 5-22. Commercial Core New Streets without Center Turn Lane Typical Cross-Section.

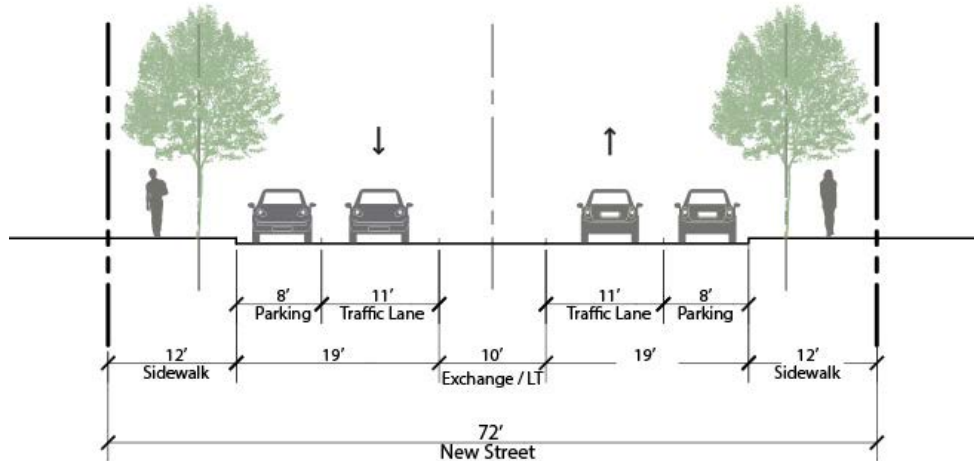


Figure 5-23. Commercial Core New Street with Center Turn Lane Typical Cross-Section.

5.9 Pedestrian Mall.

Pedestrian malls are pedestrian walkways designed for high levels of activity. In the vast majority of cases, retail uses will flank the pedestrian mall on both sides. The envisioned diagonal pedestrian mall through the Commercial Core shall be designed as part of the Compton Walk and implement the Compton Walk's design principles, adapting them appropriately for the pedestrian mall condition. The pedestrian malls shall feature outdoor lighting or uplighting to create a festive atmosphere.

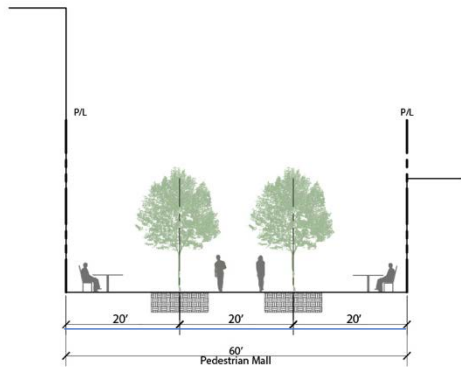


Figure 5-24. Pedestrian Mall Typical Cross-Section. Source (right): TimeOut Los Angeles

5.10 Mid-Block Passthrough.

Mid-block passthroughs are pedestrian walkways which connect streets to alleys or other streets through the middle of a block. They often provide important connections from parking structures to primary streets such as Compton Boulevard. The mid-block passthroughs will be well lit for safety.

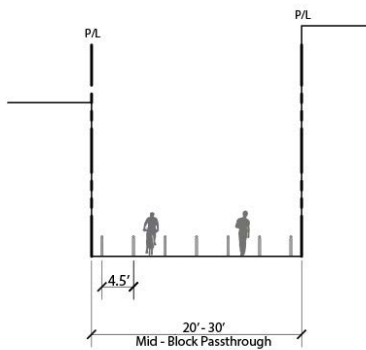


Figure 5-25. Mid-Block Passthrough Typical Cross-Section. Source (right): One Colorado Pasadena Social Media.

5.11 Shared Alley.

Shared alleys are narrow, low-speed mobility corridors which provide access for deliveries, residential vehicles accessing parking structures, and pedestrians. Because typically pedestrians and vehicles share the same space, all elements of alley design should emphasize its low-speed nature. Among these elements shall be the usage of permeable pavers for the roadbed.

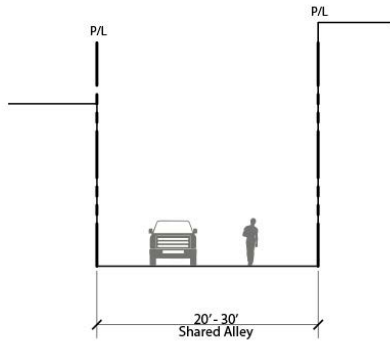


Figure 5-26. Shared Alley Typical Cross-Section.

5.12 Bicycle Network

The Compton Station Specific Plan bike network is comprised of both on-street and off-street facilities used to meet the demands of bicycle travel. A comprehensive network can improve bicycle safety and convenience, especially when coupled with education and enforcement programs. The following describes the standard hierarchy of bikeway designations within the Specific Plan area:

- Class I. Typically called a “bike path,” a Class I bikeway provides bicycle travel on a paved right-of-way completely separated from any street or highway.
- Class II. Often referred to as a “bike lane,” a Class II bikeway provides a striped and stenciled lane for one-way travel on a street or highway.
- Class III. Generally referred to as a “bike route,” a Class III bikeway provides for shared use with pedestrian or motor vehicle traffic and is identified only by signs and/or pavement markings (i.e., sharrows).
- Class IV. Known as “cycle tracks” or “separated bikeways,” these facilities provide a right-of-way designated exclusively for bicycle travel adjacent to a roadway that is protected from vehicular traffic through a physical barrier, such as landscaping or bollards.



Class I (left) and Class II (right) bicycle facilities.

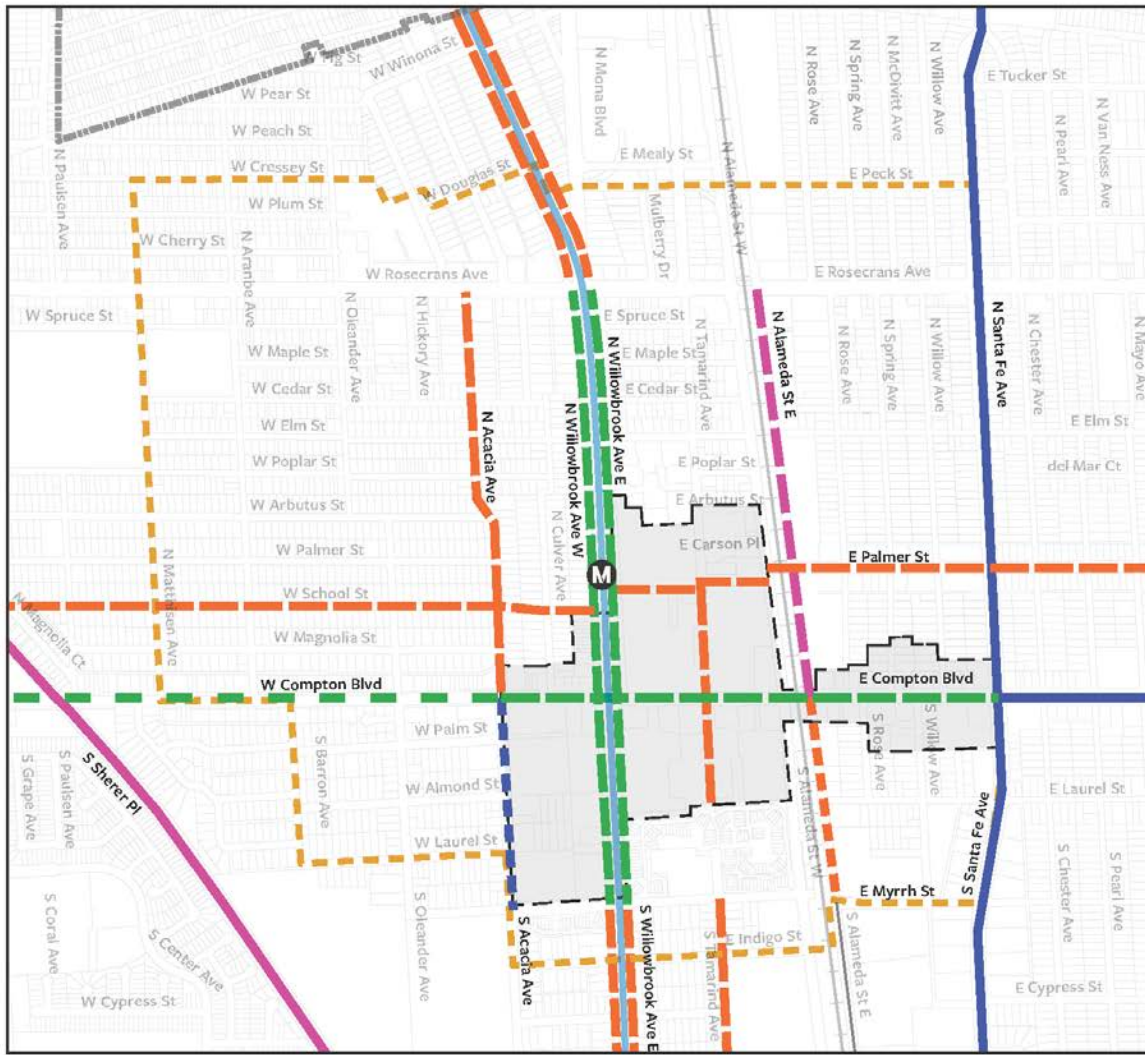


Class III (left) and Class IV (right) bicycle facilities.

Current bicycle facilities within the Specific Plan area include Class II bike lanes on Compton Boulevard east of Alameda Street in both directions as well as on Santa Fe Avenue. On Santa Fe Avenue, there are clearly marked Class II bike lanes in both directions south of Laurel Street, while north of Laurel Street there is a clearly marked Class II bike lane in the southbound direction and an implied bike lane inside of a wide curbside parking lane in the northbound direction. Clearly marked Class II bike lanes resume in both directions on Santa Fe Avenue north of Rosecrans Avenue. In addition, a shared use Class I path for bicycles and pedestrians follows along Compton Creek to the southwest (just outside the Specific Plan area).

The Specific Plan would enhance the existing network and develop new, feasible facilities to provide a bikeway system that serves to link people with schools, parks, employment and shopping areas, and transit. The key features of the bicycle network plan include the following:

- Adding a Class IV separated bikeway along Compton Boulevard through the area, where the bike lane would run adjacent to the curb, protected from vehicle traffic by on-street parking and a raised buffer. The bikeway configuration would be achieved by reducing the number of vehicle travel lanes to one lane in each direction. Vehicle capacity is available on parallel routes such as Alondra Boulevard, Myrrh Street, Palmer Street, and Rosecrans Avenue, to accommodate potential diversion of vehicle traffic.
- Adding Class IV separated bikeways along Willowbrook Avenue East and West between Rosecrans Avenue and Myrrh Street, as part of the reverse couplet configuration where Willowbrook West would run northbound and Willowbrook East would run southbound. The one-way bikeway on Willowbrook West would be separated from vehicle travel by a buffer consisting of collapsible bollards. Along Willowbrook East, a two-way bikeway could be established, at a minimum in the portion between the Metro station and Compton Boulevard.
- Adding a Class II bike lane along Acacia Avenue between Compton Boulevard and Myrrh Street. This would be achieved by reducing the number of vehicle travel lanes to one lane in each direction.
- Adding a Class I bike path along Alameda Street East between Rosecrans Avenue and Compton Boulevard. The bike path and pedestrian walkway would replace the two-lane roadway and vehicle parking within this segment.
- Finally, adding Class III bike routes, generally consistent with the Compton Bike Master Plan, are planned for Palmer Street, Acacia Avenue north of Compton Boulevard, School Street, the Tamarind Avenue extension, Rosecrans Avenue north of Willowbrook Avenue, and Alameda Street East between Compton Boulevard and Myrrh Street.



Bike Network

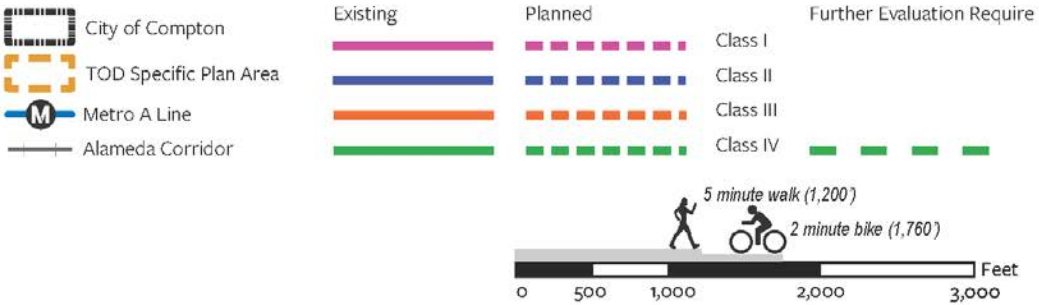


Figure 5-27. Bicycle Network.

5.13 Vehicular Access and Gateways

Figure 5-28 shows the primary vehicular access roadways into Downtown Compton and the recommended location for gateways, remote wayfinding signage, and public-serving parking structures.



Figure 5-28. Downtown Access and Gateways.

5.14 Parking Context and Parking District Strategy

The Compton Station Specific Plan shall establish a parking district within the Downtown Area to encourage the efficient use of both existing and future parking facilities, and encourage a “park once” environment where visitors, employees, and residents are able to park their cars, then walk to multiple destinations. Parking would be provided through a combination of structured facilities and surface level parking lots.

The existing parking structures in the area include the Compton City Hall/Civic Center Parking Structure (1,034-space capacity) and the Dollarhide Recreation Center Structure (466-space capacity). The Civic Center structure is located at the northeast corner of the Acacia Avenue/Myrrh Street intersection, and currently serves the employees and visitors to City Hall and the Courthouse. The Dollarhide structure is located adjacent to the Metro A Line Compton Station and currently serves visitors to the Douglas F. Dollarhide Community Center and Metro A Line patrons. Both of these existing facilities shall be utilized to facilitate parking demand as a result of the Specific Plan buildout.

The Civic Center structure is primarily utilized on weekdays only, during daytime hours. Thus, opportunities exist for shared parking usage in the evenings and on weekends. New parking demand from residential and hotel uses planned in the Civic Center area may utilize the available parking, generally on the upper floors of the structure, depending on future negotiation with the Judicial Council which controls the garage.

The Dollarhide structure is currently only approximately 20% occupied during peak weekday hours. As a result, this facility can be utilized for visitor parking for the adjacent Renaissance area development. The courthouse parking structure is operated by Los Angeles County, thus leasing agreements would be required in order to allocate spaces for particular uses.

New public parking structures would be provided in multiple locations within the Downtown area. The new structures would be located to allow for visitors to park once and be able to access multiple destinations by walking. In addition, the new structures would be set back from Compton Boulevard such that no new driveways/curb cuts along Compton Boulevard would be needed and the facilities would not be visible from the street level. Rather, vehicle access would be generally provided off of north-south streets such as Alameda Street West and new Tamarind Avenue extension. The public parking structures may also provide shared residential and employee parking for adjacent new residential and office sites, generally on the upper levels of the structures.

In addition to the public structures, new public surface or structure parking lots will be provided within the Town Center area to accommodate demand. The City can also explore acquiring existing parking lots or vacant lots for public surface parking lots to serve other areas of the downtown as needed.

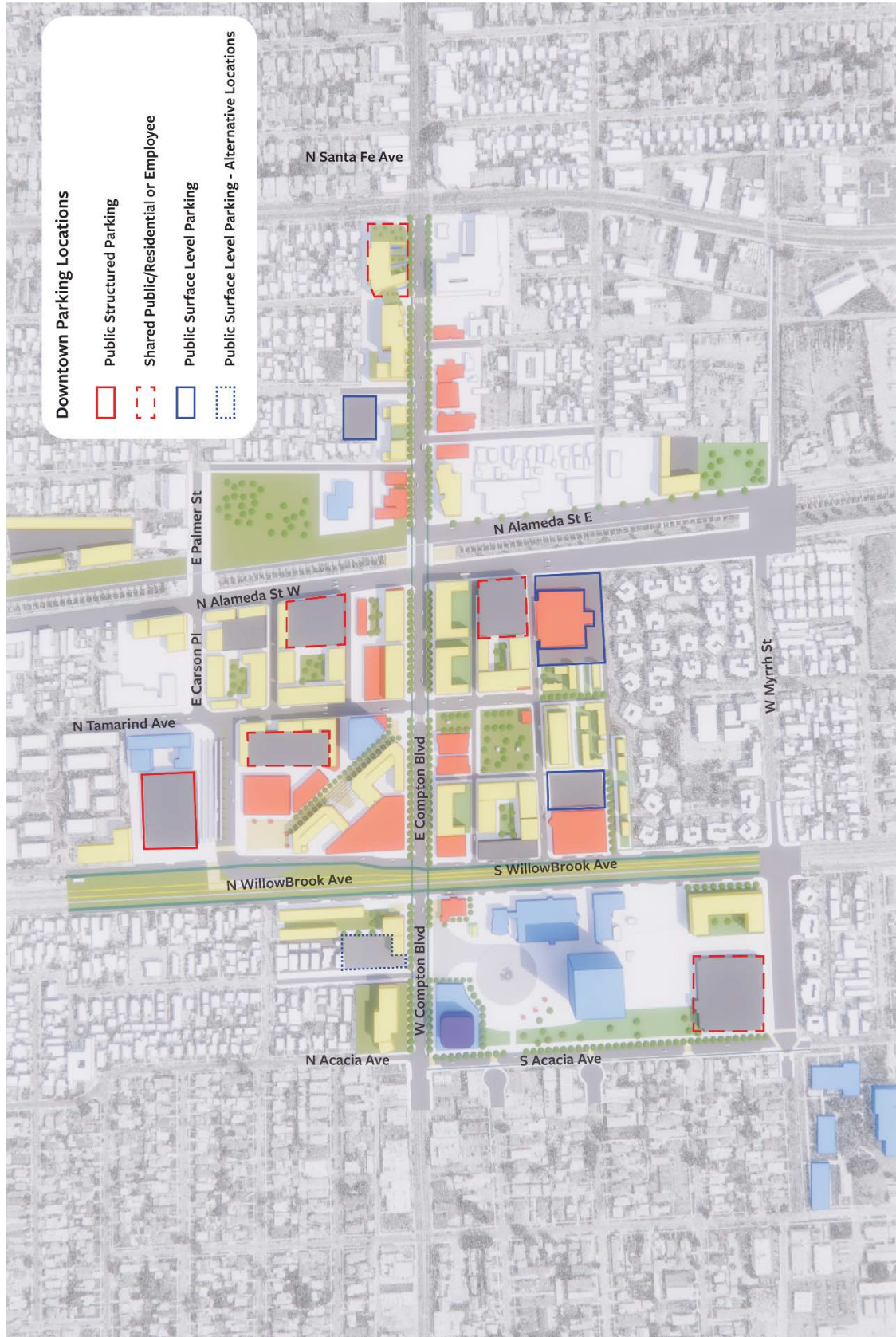


Figure 5-29. Downtown Parking Locations.

5.15 Parking and Transportation Demand Management (TDM) Regulations

1. Parking District
 - A. Purpose. A parking district shall be created within Downtown Compton to encourage the efficient use of existing and new parking resources, and encourage a “park once” atmosphere where visitors and employees in the area park their cars, then walk to multiple destinations.
 - B. As many spaces as are designated for use of retail, food service or other similar public-serving establishments shall not be restricted from serving patrons of commercial businesses in the area proximate to the subject development. Time limits and parking fees may be applied.
2. Parking Unbundling
 - A. The cost of parking spaces in the Downtown area shall be unbundled from the cost to rent residential or commercial space in the building. For example, residents must be given the option to rent no spaces, one space, or if there is sufficient supply, two parking spaces at differing prices.
3. Special Vehicle Parking Requirements

All developments shall provide:

 - A. Vehicle charging station for a minimum of one percent of the total number of vehicle parking spaces provided;
 - B. Designated stalls for scooters, mopeds, and motorcycles at a ratio of one space for every 25 units and/or 25,000 square feet. These spaces shall have a minimum dimension of 5’ x 8’, and they shall count toward satisfying the minimum parking requirement.
 - C. Directional signage at all parking area entrances indicating the location of vehicle charging stations, shared vehicle parking spaces, and the stalls for scooters, mopeds, and motorcycles
4. Parking Lifts and Tandem Parking
 - A. Use of any type of parking lifts that allow for motorists to retrieve a vehicle without having to have another person move their vehicle first is permitted.
 - B. Tandem parking is only permitted between residential spaces belonging to the same unit. Both tandem spaces shall be 22 feet long.
5. Off-Site Parking
 - A. The number of parking spaces required is given in Figures 3-5, 3-8, 3-11, 3-14 and 3-17.
 - B. Parking required to serve a nonresidential use may be on the same or a different site under the same or different ownership as the use served, provided the parking shall be within 300 feet of the use served, on the same block or crossing only an alley or local street. Said distance shall be measured along public walkways from the nearest lot line of the lot on which the uses are located to the nearest lot line of the lot on which the required parking is located.
 - C. Required nonresidential parking may be located off-site from the use which it serves, subject to the approval of the Community Development Department Director of a lease agreement and recorded covenant and recorded covenant providing that the off-site parking shall be available when the use commences and continuing so long as the use is in effect.
 - D. A Building Permit for the use shall not be issued until the covenant has been recorded with the County Recorder and a copy filed with the Community Development Department. The covenant cannot be terminated, amended, or removed without the written approval of the Community Development Department.

- 6. Parking Structure Design
 - A. Ground floor parking structure façades facing public streets shall be limited to entrance and exit locations and comply with the standards of the Parking Frontage (Figure 3-23).
 - B. Standalone Structures. Standalone parking structures shall meet the following standards which are intended to facilitate potential adaptive reuse of the structure for other purposes if the parking need decreases in the future.
 - I. Ground floor height shall be a minimum of 15 feet measured from floor to ceiling.
 - II. Upper floor heights shall be a minimum of 10 feet measured from floor to the lowest structural member of the ceiling.
 - III. Light wells shall be provided between parking bays and shall have a minimum width of 25 feet.
 - IV. All floors of the parking structure shall be designed as flat, level surfaces.
 - V. The structural calculations for the floors within the parking structure shall be designed to take into account the need for additional load-carrying capacity for future habitable uses.
 - VI. Levels of parking above the ground floor shall be architecturally enhanced to minimize the visual impact of above ground parking structures from street views along collector and arterial streets.
 - C. Alternative Compliance. Alternative compliance to (B) may be achieved through the construction of a below grade parking structure or a lower-floor podium of a multistory building.
- 7. Bicycle Parking
 - A. Bicycle parking rates shall comply with Figure 5-30.

Minimum Bicycle Parking Requirements	
Use	Number of Spaces
Multi-Family ¹	0.5 space per unit
Restaurants, Bars and Clubs	1 per every 300 sf
Retail sales and services uses; offices ¹ ; and entertainment uses, excluding theaters	1 per every 1,000 sf; 3 min.
Theaters	1 space per 250 sf
End Note: ¹ Multi-family developments and office developments are required to have both short-term guest parking racks and long-term resident/employer parking enclosures.	

Figure 5-30. Minimum Bicycle Parking Requirements.

- B. Bicycle Parking Design. Bicycle parking spaces shall comply with the standards in this Subsection.
 - I. Bicycle parking shall consist of either a rack or lockable enclosure with individually secured spaces;
 - II. Lockers and racks shall be securely anchored to a structure or the pavement;
 - III. Racks shall be designed and installed to support the bicycle upright by its frame in 2 places in a manner that will not cause damage to the wheels and to allow the frame and one or both wheels to be secure;

- IV. When located within a parking area, curbs, fences, landscaped areas, or similar barriers shall be installed and maintained for the mutual protection of bicycles, vehicles, and pedestrians, unless the Community Development Department Director or designee determines it to be unnecessary; and
 - V. Customer and guest bicycle parking shall be placed in a convenient, highly-visible, active, and well-lit location not more than 100 feet walking distance on the ground floor from the main entrance but may not interfere with pedestrian movements.
 - VI. Resident and employee bicycle parking shall be placed in a sheltered and secured area with individually secured spaces.
- 8. Transportation Demand Management.

This section overrides the code text in CMC 12-7-2:

 - A. Any development of over 50,000 total square feet shall provide, in the lobby or other location visible to the greatest number of project employees or residents up to date and printed information about public transportation, carsharing and bicycling options in the area, such as maps, advertisements for useful digital applications, etc. Printed information regarding on-site facilities such as bicycle parking and showers, if applicable, shall also be provided. In addition, such developments must upgrade bus stops adjacent to the property or within 100 feet of any property line to be compliant with Metro's Transfers Design Guide or other standards of the stop's transit operator. Bus stops shall at a minimum include a shade structure and bench. The Director of Public Works shall have the ability to waive this requirement if it is infeasible.
 - B. Any development of over 100,000 total square feet shall comply with the requirements of (A) and (B) and maintain carshare vehicles on-site. At least one carshare vehicle must be made available for each 100 residential units or 100 anticipated project employees. Shared company vehicles may substitute for carshare vehicles in proportion to the number of employees that they are available to.
- 9. Shared Parking and Parking Reductions. When the Director of Community Development determines that the peak parking demand for a project may be met by a number of parking spaces less than required by CMC 30-21, he or she may grant a reduction of the total number of parking spaces by up to 50 percent, provided that one or both of the following conditions are met:
 - A. Two (2) or more uses share a parking facility and satisfy the requirements below:
 - I. The applicant shall provide a parking study from a professional engineer (PE) or traffic engineer (TE) based on a survey of at least three similar projects.
 - II. Each applicant that desires to participate in a shared parking program shall show evidence that there is no substantial conflict or overlap in the principal operating hours of the buildings or uses for which the shared parking facilities are proposed.
 - III. Parking facilities may be located on a different lot from the lot on which the use to be served is located provided the parking is on the same block or crossing only an alley or local street and the lot used for the required parking is not greater than three hundred (300) feet. Said distance shall be measured along public walkways from the nearest lot line of the lot on which the uses are located to the nearest lot line of the lot on which the required parking is located. Property owners involved in the joint use of off-street parking facilities shall submit an agreement and covenant for such joint use by a proper legal covenant approved by the Director as to form and content. Such covenant shall be submitted with the entitlement application. Such instrument, when approved as conforming to the provisions of this Section, shall be recorded in the office of the County Recorder within thirty (30) days of City approval, and copies of said recorded documents filed with the

Community Development Department.

- B. Applicants may provide Additional Transportation Demand and Trip Reduction Measures in excess of those required in (8) above, contingent upon the following requirements:
- I. The applicant shall produce a parking study from a professional engineer (PE) or traffic engineer (TE) demonstrating that the Additional Transportation Demand and Trip Reduction Measures will be effective in reducing on-site parking demand.
 - II. Examples of additional Transportation Demand and Trip Reduction Measures include but are not limited to providing free transit passes and free carshare memberships to project employees or residents; establishing a “Guaranteed Ride Home” program and promoting work from home.

Chapter 6: Infrastructure

6.1 Introduction

The purpose of this report is to provide an overview of existing and planned infrastructure facilities, and to outline recommended infrastructure upgrades for the City of Compton TOD Specific Plan. The recommended upgrades are based on the analysis of the existing utilities /infrastructure within the Transit Oriented Development (TOD) project area which are based upon the information gathered from the City of Compton's Public Works Department and private utility purveyors, and the development capacity shown in Figure 6-1.

The study area encompasses public/non-government institutions, residential and commercial properties, industrial, commercial/limited/heavy manufacturing, and mixed use. The available utility infrastructure has little constraints regarding future redevelopment.

The highest density demand allowed by the city is 175 du/acre, although most development is not anticipated to reach that density.

Study Area: A

This area encompasses the north corridor from North of Rosecrans Avenue and between N. Matthisen Avenue eastbound to N. Willow Avenue.

Study Area: B

This sector runs along E. Rosecrans Avenue, from N. Matthisen Avenue to N. Santa Fe Avenue.

Study Area: C

The sector is bordered by N. Matthisen Avenue to N. Willowbrook Avenue to the west and east and W. Spruce Street, and W. Palmer Street to the north and south, respectively.

Study Area: D

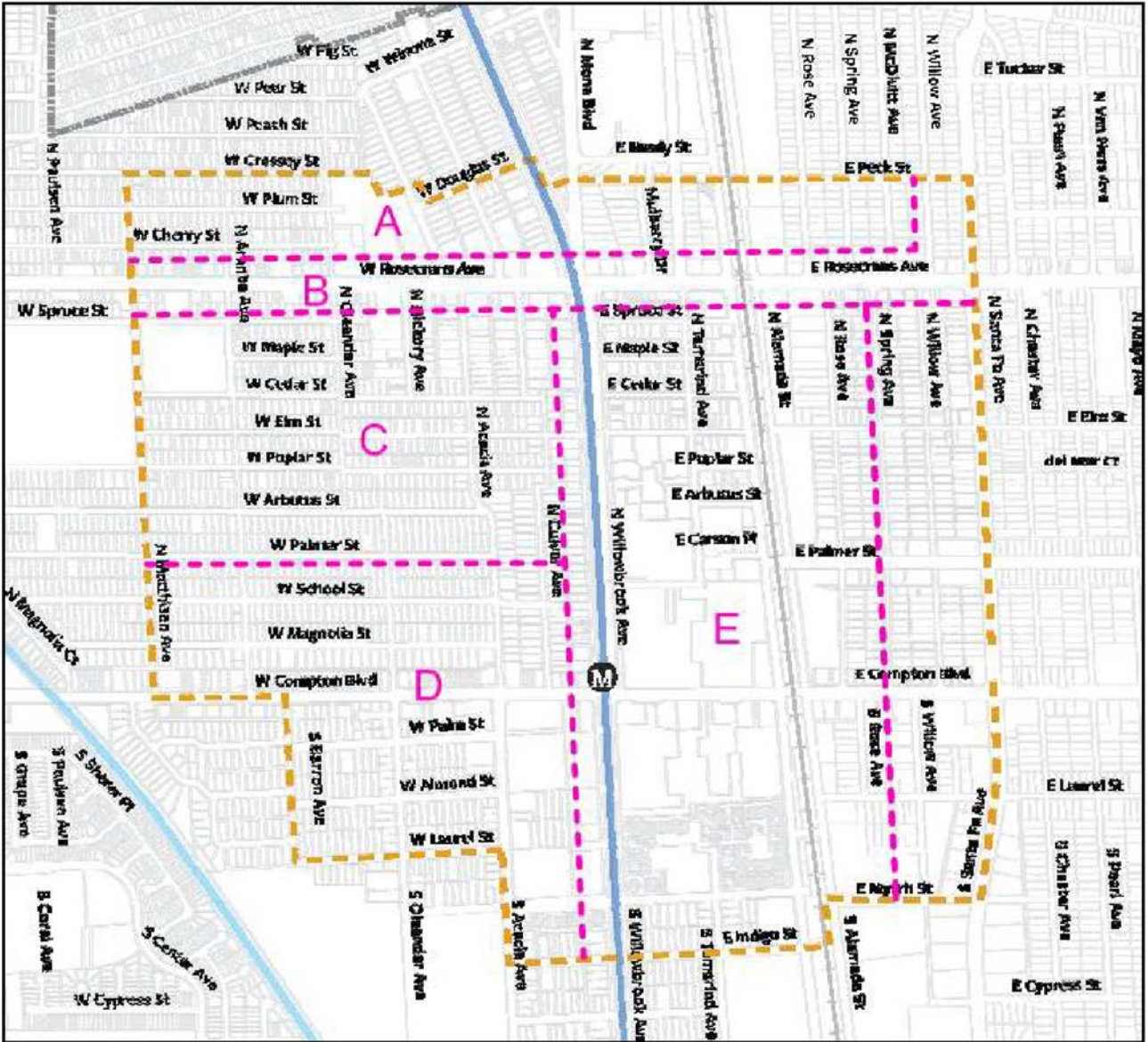
This sector is bounded by W. School Street to the north and S. Willowbrook Avenue to the east. Starting southbound on N. Matthisen avenue to eastbound on W. Compton Boulevard to southbound on S. Barron Avenue to eastbound on W. Laurel Street to southbound on N. Acacia Ave to eastbound on W. Indigo Street.

Study Area: E

This sector is bounded by N. Willowbrook Ave to the west and N. Spring Ave to the east, E. Spruce to the north and W. Indigo St. to the south.

Study Area: F

This sector is bounded by N. Willow Ave to the west and N. Santa Fe Ave to the east, south of Rosecrans Ave., and north of E. Myrrh St.



-  City of Compton
-  TOD Specific Plan Area
-  Metro Line A
-  Alameda Corridor
-  Compton Creek
-  Downtown
-  Sub areas

Figure 6-1. Development Capacity Map.

Development Capacity						
Location	Existing Residential (units)	New Residential (units)	Total Residential (units)	Existing Non-Residential (sf)	New Non-Residential (sf)	Total Non-Residential (sf)
Area A	379	15	394	40,953	0	40,953
Area B	179	125	304	197,042	0	197,042
Area C	873	45	918	69,457	0	69,457
Area D	894	252	1146	199,112	100,000	299,112
Area E	877	1,631	2,508	644,649	48,000	692,649
Area F	364	110	474	225,057	0	225,057
Total:	3,566	2,178	5,744	1,376,270	148,000	1,524,270

Figure 6-2. Development Capacity by Infrastructure Analysis Area.

6.2 Water

6.2.1 Existing Conditions

The City of Compton services approximately 80% of the City's residents through more than 15,000 individual water connections. The City of Compton's Water Utility Division oversees the largest network of water infrastructure, approximately one hundred and fifty-six (156) miles of pipe in length. The drinking water is a blend of groundwater from the Central Basin groundwater basin and surface water imported by the Metropolitan Water District (MWD). The water imported by the MWD is a blend from Northern California's State Water Project water and from the Colorado River Aqueduct. Also, the City has adjudicated rights to pump 70% of the City's water demand. The majority of the City's water utility network is comprised of pipes made of Asbestos-cement (Transite) pipes, Ductile Iron (DI) pipes and Cast Iron (CI) pipes and range from 4"-16" in diameter.

Study Area A: The water mains range from 4"-12" in diameter and comprised of TR, CI, and DI lines.

Study Area B: The water mains range from 4"-16" in diameter and comprised of TR, CI, and DI lines.

Study Area C: The water mains range from 4"-10" in diameter and are TR, CI, and DI lines.

Study Area D: This sector of the study area is the southwest quadrant, south of W. Palmer Street and west of S. Willowbrook Ave. The water mains range from 4"-12" in diameter and are TR, CI, and DI lines.

Study Area E: The water mains range from 4"-12" in diameter and are TR, CI, and DI lines.

Study Area F: The water mains range from 4"-12" in diameter and are TR, CI, and DI lines.

6.2.2 Recommendations

Currently the City's water system can accommodate existing demands and they do not anticipate any issues with meeting future water demands.

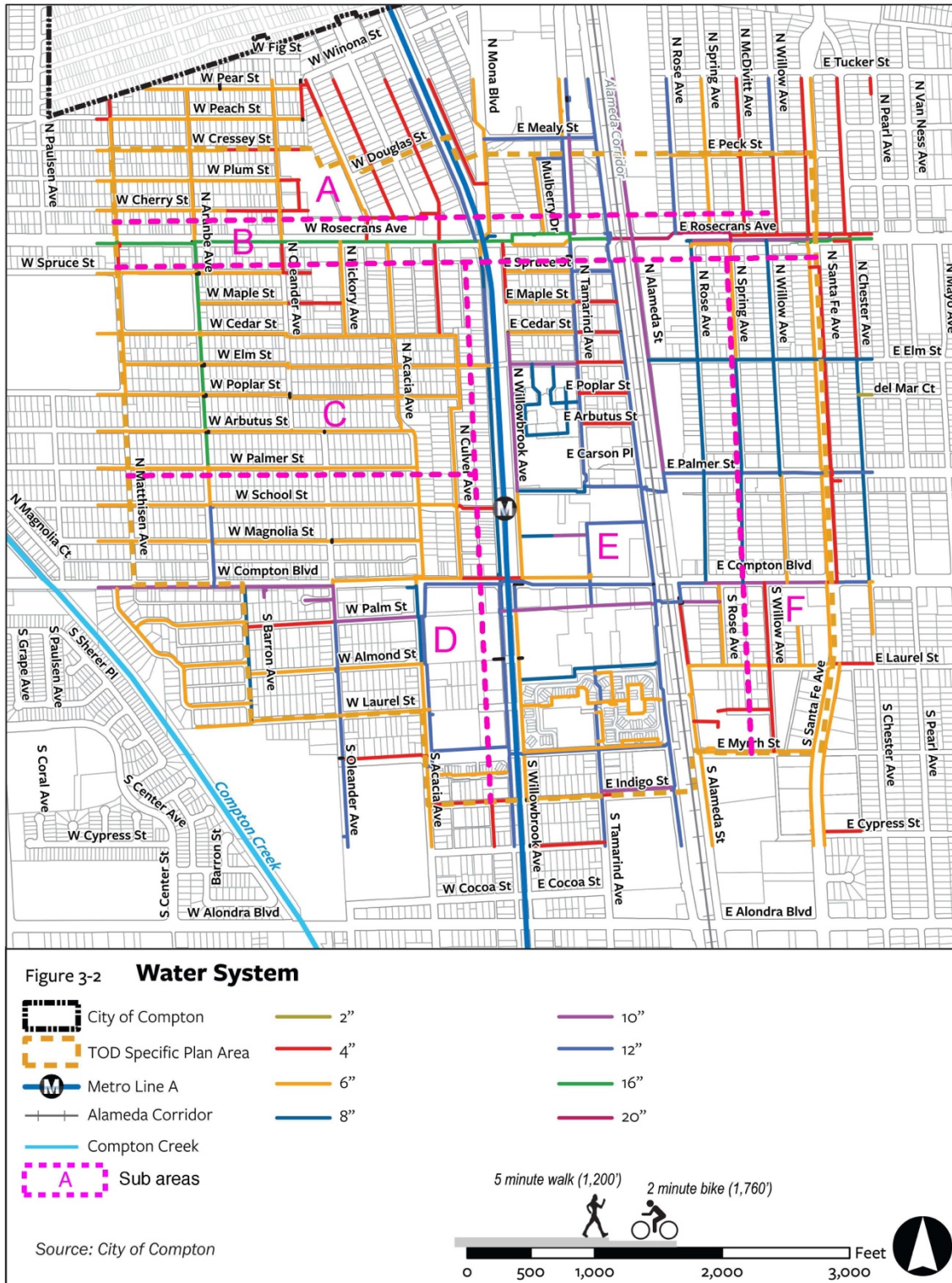


Figure 6-3. Water System.

6.3 Sewer

6.3.1 Existing Conditions

The City's Public Works department owns and maintains the existing sewer system, which is a comprehensive network. Majority of the existing sewer mains are 8" lines but also does range from 6"-12" in diameter. The sewer pipe material for most of the network within the study area is unconfirmed.

Study Area A: The sewer mains range from 6"-10" in diameter and are reinforced concrete pipelines.

Study Area B: The sewer mains range from 8"-10" in diameter and are reinforced concrete pipelines.

Study Area C: The sewer mains range from 8"-12" in diameter and the material is mostly unconfirmed and reinforced concrete pipelines.

Study Area D: The sewer mains range from 8"-15" in diameter and the material is mostly unconfirmed and reinforced concrete pipelines.

Study Area E: The sewer mains range from 8"-15" in diameter and the material is mostly unconfirmed and reinforced concrete pipelines.

Study Area F: The sewer mains range from 8"-15" in diameter and the material is mostly unconfirmed and reinforced concrete pipelines.

6.3.2 Recommendations

Based on the analysis on the capacity and capability to meet future demand of the City of Compton study area, an assessment of the existing sewer line was re-evaluated for the increase demand and the outcome of the analysis are as follows:

Study Area A – There is no future development on this area.

Study Area B – The existing sewer mains are capable to carry additional capacity of the future development in this area.

Study Area C – The existing sewer mains are capable to carry additional capacity of the future development in this area.

Study Area D – Future residential development will occur at the residential zoning area within the study area. Additional new development also includes hotel and office building which will be constructed at the commercial zone along the Compton Boulevard. The additional sewer flow for these developments will tap into the existing sewer lines at the residential and commercial areas which are mostly 8-inch RCP and onto the 15-inch RCP main sewer line at S. Oleander Avenue which is where all sewage generated by the new developments will discharge to. Therefore, the existing sewer mains are capable to carry additional capacity of the future developments.

Study Area E – Future residential development will occur at the residential zoning area within the

study area. Envisioned new development also includes an office building which will be constructed along the Compton Boulevard. The additional sewer flow for these developments will tap into the existing sewer lines at the residential and commercial areas which are mostly 8-inch RCP and onto the 10-inch RCP sewer line at N. Tamarind Avenue and at the 15-inch RCP main sewer at S. Willowbrook Avenue which is where all sewage generated by the new developments will discharge to. Additional sewer flow for future commercial developments at the Renaissance Plaza and Towne Center will tap into the existing 15-inch sewer line at Douglas Dollarhide Drive which is where all sewage generated by these new developments will discharge to. Therefore, the existing sewer mains are capable to carry additional capacity of the future development.

Study Area F – The existing sewer mains are capable to carry additional capacity of the future development in this area.

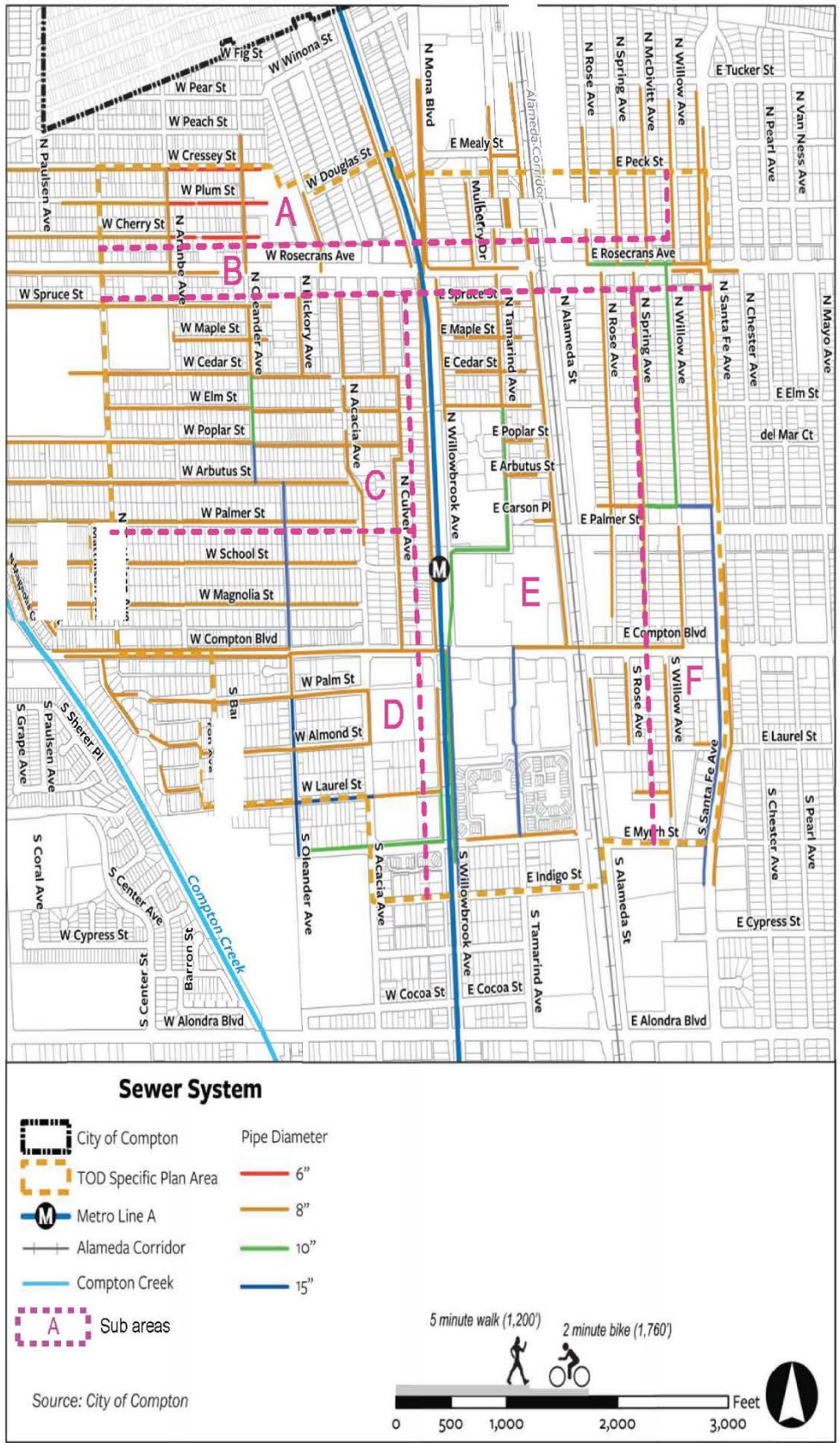


Figure 6-4. Sewer System.

6.4 Storm Drain

6.4.1 Existing Conditions

The Los Angeles County Storm Drain System (LACFCD) is responsible for the management of the existing storm drain system, which is a comprehensive network. The existing trunkline Glen Avenue drainage system located at Mona Blvd and Willowbrook Avenue, BI 0006 - Unit 2 Line E, BI 0006 - Unit 1 Line A and BI 0421 Line E located at Santa Fe Avenue are the main storm drain system. The City's Public works department is also responsible for the maintenance of small lateral pipes and other storm drains within the city.

Study Area A: The stormwater mains range from 39", 60" and 108" in diameter and are reinforced concrete pipelines.

Study Area B: The stormwater mains range from 24", 27", 39", 48", 60", 66" and 108" in diameter and are reinforced concrete pipelines.

Study Area C: The stormwater mains range from 24", 27", 30" and 36" in diameter and are reinforced concrete pipelines.

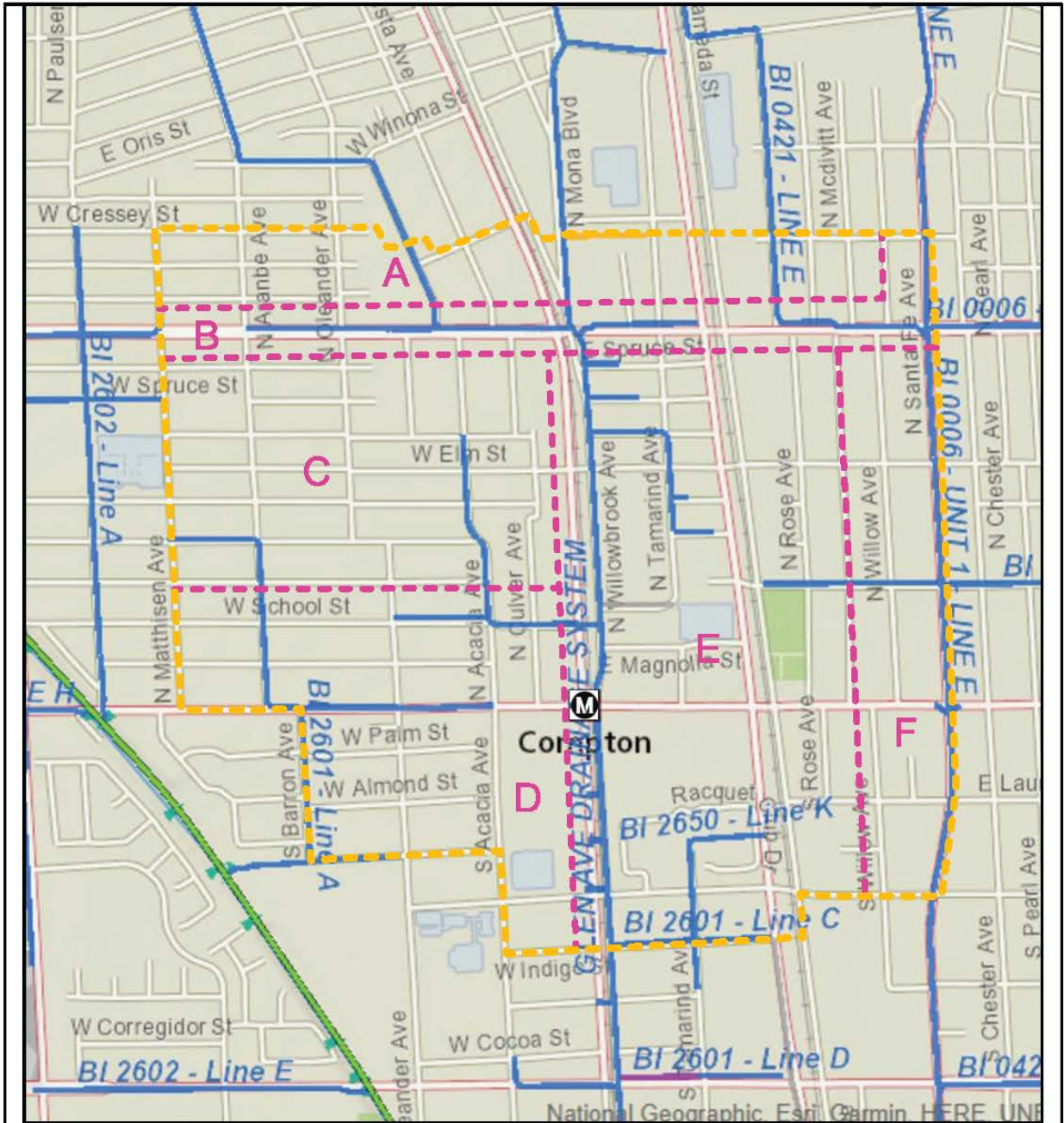
Study Area D: The stormwater mains range from 30", 42" and 48" in diameter and are reinforced concrete pipelines.

Study Area E: The stormwater mains range from 24", 33", 39", 42", 54", 108" and 138" in diameter and are reinforced concrete pipelines.







Study Area F: The stormwater mains range from 42", 60", 108" in diameter and are reinforced concrete pipelines.

6.4.2 Recommendations

Upgrading of the existing storm drain system will require a further study in the specific plan and an Environmental Impact Report (EIR) will analyze the existing infrastructure. This is according to the City of Compton Public works department Senior Planner managing the TOD project. They are not yet on the level of detail in the specific plan, so there are no immediate recommendations thus far to upgrade the existing storm drain system within the City of Compton TOD Study Area.



Storm Drain System

-  City of Compton
-  TOD Specific Plan Area
-  Metro Line A
-  Alameda Corridor
-  Compton Creek
-  Sub areas

 Storm Drain System

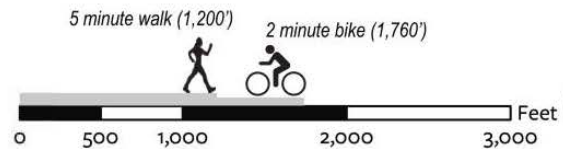


Figure 6-5. Storm Drain System.

6.5 Electrical

6.5.1 Existing Conditions

The Southern California Edison (SCE) Company is the electrical purveyor for the City of Compton. Edison's existing electrical facilities are mostly overhead and underground conductors. The overhead conductors share poles with telecommunications and cable TV facilities.

6.5.2 Recommendations

The decision to upgrade the power supply facilities and the number of upgrades to meet the demand of future development will be determined by SCE in coordination with the city after developers have submitted their building plans. Demand for services and the ability to serve new developments are generally determined on a case-by-case basis.

Most of the electrical distribution facilities in the City of Compton's study area are aerial facilities. It is recommended that existing aerial electrical facilities be placed underground whenever funding is available. Underground electricity provides higher reliability, is safer in general and is also less unsightly. One of the potential funding mechanisms is CPUC Rule 20. CPUC Rule 20 is a set of policies and procedures established by the CPUC to regulate the conversion of overhead electric equipment to underground facilities, a process often referred to "undergrounding". Rule 20 determines the level of ratepayer funding for different undergrounding arrangements.

CPUC Rule 20

Under Rule 20, undergrounding projects are financed by utility rate money, combined rate funds and local tax proceeds, or private funds, depending on whether Rule 20A, Rule 20B or Rule 20C provisions apply.

Rule 20A: *Rule 20A projects are paid for by all SCE customers and ratepayers, not just those who live in locations where facilities will be undergrounded. To qualify for full funding through utility rate proceeds, projects must produce a benefit to the general public, not just customers in the affected area, by satisfying one or more of these criteria:*

- *The location has an unusually heavy concentration of overhead facilities.*
- *The location is heavily traveled.*
- *The location qualifies as an arterial or major collector road in a local government's general plan.*
- *The overhead equipment must be located within or pass through a civic, recreational, or scenic area.*

Using CPUC formulas, SCE allocates rate funds to communities for undergrounding based on previous allocations, the ratio of customers served by overhead facilities to all the customers in the community, and the fraction that customers in the community represent of all SCE customers. Local governments use these formulas to project allocations, which allow them to prioritize projects and develop project schedules. Because funds are limited, local governments sometimes must wait and accumulate their allocations before starting an undergrounding project.

Rule 20B: *If an area is not eligible for Rule 20A or if local government cannot or chooses not to rely on the Rule 20A allocation process, Rule 20B allows rate funds to subsidize an undergrounding project. The subsidy includes an amount equal to about 5-20% of the total cost. The remaining cost is funded by local governments or through neighborhood special assessment districts.*

Rule 20C: *Rule 20C enables property owners to pay for undergrounding electric lines and equipment if neither Rule 20A nor 20B applies.*

It is also worth noting that, prior to any undergrounding process, SCE will take the lead in contacting the other low voltage utilities that might be sharing the power poles with SCE aerial facilities, such as the telecommunication providers and cable TV provider to coordinate and execute a joint trench. During this process, SCE will also reach out to the City's Public Service Counter for their assistance in contacting and coordinating with the aforementioned utility providers.

It is recommended that prior to approving new development, the City should contact SCE regional manager for the City of Compton study area and set up an inspection at the new development site in order to determine whether the existing aerial SCE facilities in the area qualifies for Rule 20A funding.

6.6 Natural Gas

6.6.1 Existing Conditions

Southern California Gas Company is the only natural gas purveyor for the residents and businesses within the City of Compton. Almost every parcel within the study area is connected to the SoCalGas facilities. The existing gas mains are predominantly galvanized steel lines with a few polyethylene (PE) and cross-linked polyethylene (PX) lines which range from 1"-8" in diameter.

Study Area A: The gas main ranges from 2"-6" in diameter and are galvanized steel and cross-linked polyethylene lines.

Study Area B: The gas main ranges from 2"-4" in diameter and are galvanized steel and cross-linked polyethylene lines.

Study Area C: The gas main ranges from 2"-4" in diameter and are galvanized steel lines.

Study Area D: The gas main ranges from 2"-8" in diameter and are galvanized steel and cross-linked polyethylene lines.

Study Area E: The gas main ranges from 2"-8" in diameter and are galvanized steel and polyethylene lines.

Study Area F: The gas main ranges from 1"-7" in diameter and are galvanized steel lines.

6.6.2 Recommendations

The analysis on the capacity and capability to meet future demand will be conducted by The

Southern California Gas Company (SoCalGas) in coordination with the County upon submittal of building plans by developers.

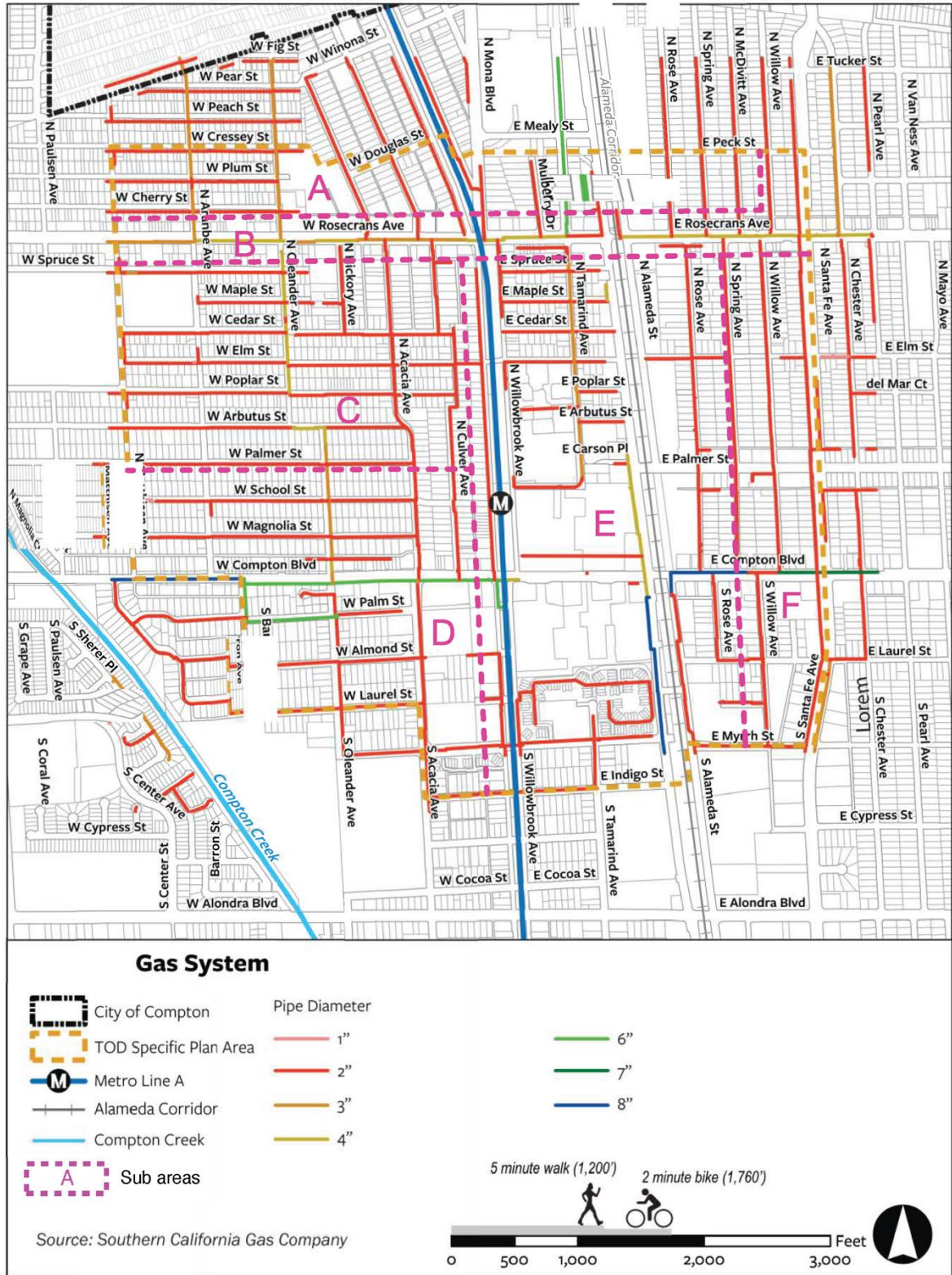


Figure 6-6. Gas System.

6.7 Telecommunications

6.7.1 Existing Conditions

Level 3 Communications was a multinational telecommunications and internet service provider, the 3rd largest provider of fiber-optic internet access (based on coverage) in the United States. Level 3 was acquired by CenturyLink on November 1, 2017.

Study Area E: CenturyLink's only fiber optic line in the City of Compton runs along Alameda Street West to Los Angeles and continues along the railroad.

6.7.2 Recommendations

The local telecommunication system providers will assess the demand for services and the ability to serve new developments on a case-by-case basis. The capacity and capability analysis for meeting future demands within the study area will be conducted after building plans are submitted by developers.

If there is "undergrounding" of existing aerial telecommunication facilities, SCE will reach out to CenturyLink coordinate a joint trench. To save on costs, the telecommunication company is likely to participate in the joint trench operation. SCE will dictate the layout of the trench, and the telecommunication lines would follow.

Chapter 7: Implementation and Financing Strategies

7.1 Implementation and Financing Strategies

The transformation of Downtown Compton into a world-class destination that the entire Compton community can enjoy is a multifaceted endeavor which will require diligence, creativity, partnership and sustained will. Many actions will be required on the part of different City departments, private landowners, and community stakeholders. However, this chapter focuses on key features of the strategy which include:

- Establishing a property-based Business Improvement District (PBID) to maintain, secure program and park the Downtown area (Section 7.2)
- Developing public land for affordable housing and other public benefit uses (Section 7.3)
- Foment arts and culture through the Compton Walk and community collaborative arts projects, and engage philanthropic support (Section 7.4)
- Establish local historic designation (Section 7.5)
- Pursue local and outside funding sources (Section 7.6)
- Construct capital projects with help of outside funding sources (Section 7.7)

7.2 Downtown Compton Business Improvement District

A Business Improvement District (BID) is created to perform a variety of services to revitalize and improve commercial neighborhoods under a public-private partnership framework. Under California law, two different types of BIDs are permitted. The first one is created through assessments on businesses. The second, called a property-based BID or PBID, is created through assessments on property owners alone. BIDs allow business or property owners to collectively pay for services to augment those provided by the local government. BIDs are popular because business and property owners are often more willing to assess themselves if they see direct benefit to the value of their business or property.

Because many of the businesses which are anticipated for the future of Downtown Compton are not yet located in the area, a property-based BID may be the best option to provide security, maintenance, marketing, and programming for the area. Some of the activities which could be taken on by the PBID include:

- Hiring guides to provide assistance and security for visitors to Downtown. Unarmed guides are used in many districts to monitor security, conduct outreach with unsheltered homeless persons in the area, coordinate cleaning and maintenance needs, and provide assistance to visitors who have questions. When needed, they can contact police for rapid assistance.
- Providing a superior level of maintenance to the district than through the city in general. Services such as regular power-washing of sidewalks, emptying of trash cans, and landscape maintenance would be critical in maintaining a clean and inviting district which gives visitors confidence to visit the area in the daytime and nighttime hours.
- Operating the public parking facilities described in Section 5.X, including the Dollarhide and Civic Center garages.
- Programming events such as concerts, festivals, and farmers' markets in the major public spaces of Downtown.
- Promoting Downtown as a destination. Possible actions include the development of a brand for the area, supporting the Specific Plan's goal of bringing a hotel and cultural center to downtown Compton, and potentially organizing services for tourism, such as

tours of the City which pump money into the community.

Establishing a Property-Based Business Improvement District (PBID) in Downtown Compton.

Property-owners in the PBID are assessed a fair-share allocation of 'special benefits', that is, over and above the general benefits, from proposed PBID activities. General benefits from the PBID realized by public not located in the district and/or properties adjacent to the district boundaries are backed out to estimate the total special benefit costs to be allocated to the PBID properties. These special benefit costs are then assessed to property owners based on the type of service and the estimated share of their property of these services, which is shown as a line item on their property tax bill. A variety of factors can form the basis of this allocation, for example, parcel size, building size, parcel frontage, property use etc., including some combination of these factors. Publicly-owned parcels, such as the Civic Center, will not be exempt from these assessments.

The process to establish a PBID involves the following steps:

- Following the adoption of the Specific Plan, developing a district strategic plan built on stakeholder (property-owner) inputs and consensus to describe the vision and priorities for the PBID.
- Based on this, developing a 'District Management Plan' that lays out the business plan for the PBID, including the district boundary, parcels included, type of services and improvements, costs to provide these services and improvements, estimation of special benefit costs and the calculation of special benefit assessment to each property owner.
- Completing a petition campaign and a ballot process, whereby property owners contributing 50 percent plus \$1 of the total special benefit assessments agree to petition the City to establish the PBID. Any single property that contributes more than 40 percent of the special benefit costs is excluded from the ballot, with the remaining properties determining the ballot outcome. Once a BID is formed, all property or business owners within its boundaries must pay their assessment, even if they initially opposed its formation.
- Next, the PBID property owners submit a written petition to the City, which may initiate proceedings to form a district by the adoption of a resolution expressing its intention to form a district.
- Upon approval, a non-profit PBID management organization with a board of directors is established to oversee the operations and collection of assessments from the County Assessor.
- A PBID can be approved for an initial period of 5 years, followed by a renewal of 10 years.
- The management district plan may set forth specific increases in assessments for each year of operation of the district.

7.3 Public Land Development

Many of the key projects envisioned by this Specific Plan are located on land owned by the City of Compton or County of Los Angeles. These projects are of particular importance to the implementation of the community vision because they are less responsive to market conditions and return requirements than development on privately-owned land. As a result, they are particularly oriented to serve several key purposes:

- As catalysts for development in the Specific Plan area
- For public benefit projects, such as community, arts, museum, or educational spaces which may not generate a financial return

- To provide affordable housing and affordable commercial space to establish a bulwark against displacement
- To help secure outside funds from public agencies, foundations, or other institutions

Development projects on public land which are not yet subject to an Exclusive Negotiating Agreement will be subject to the Surplus Land Act, which was strengthened in 2019. This Act requires that City-owned land which is declared surplus be offered first to developers of 100% affordable housing. As a result, all housing development anticipated on publicly-owned land is anticipated to be affordable housing. This will help to balance out the housing construction on privately-owned land, which may be affordable but is more likely to be market-rate, and as mentioned above, fulfill an important goal of the plan to minimize displacement by creating affordable housing options for Compton residents.

Government agencies at the state and County level have created several programs which encourage transit-oriented affordable housing projects on public land by providing financial assistance to the projects and/or infrastructure improvements which support these projects and transit access. Among these programs are Affordable Housing and Sustainable Communities (AHSC), Infill Infrastructure Grant (IIG) and Metro Affordable Transit Connected Housing (MATCH). For example, the Compton Innovation Hub project at 501/601 E. Compton Blvd. secured \$21 million in an AHSC grant in 2022 for affordable housing construction, Metro bus purchases, and buildout of the Compton Walk in the Alameda Street East to Santa Fe Avenue segment.

The major public land development opportunities in the Specific Plan and their associated infrastructure projects could be subject of grant applications in the future. See Figure 7-2 for a map of these infrastructure projects.

Civic Center

The 17-acre Civic Center is mostly owned by the County of Los Angeles, with City Hall and the Sheriff station owned by the City of Compton. The City of Compton will need to work proactively with the County Supervisor's office as well as its operating departments in order to orchestrate the desired change in this area.

The Urban Design Framework for the Civic Center is described in Section 2.4. There are two major vertical development project envisioned on the site. A 150-unit affordable housing project would go on the large City-owned lot occupied by the Heritage House. The Heritage House, a local historic monument, would need to be moved as illustrated in Figure 2-6 or incorporated into the design of the project. Appropriate populations could include families or small worker households. Parking could be provided onsite or in the current Civic Center garage should appropriate arrangements be made with the garage owner, and outside funding could be leveraged to support the change of Willowbrook Avenue into a couplet with bicycle facilities.

One additional option, not pictured in the Urban Design Framework, is to free up extra land by providing the Sheriff's Department with new facilities off-site in an area more suited to rapid vehicular entry and exit than Downtown Compton will become. One potential area for this is in the Rosecrans Center district near the intersection of Rosecrans Avenue and Alameda Street; however, there may be other opportunities in the City as well. With extra land available on the Civic Center site, space may be available for a larger development which could be entirely affordable or mixed-income as needs dictate.

A second development potential is a hotel and cultural center/museum project proposed on the site of the current library on the northwest side of the MLK Memorial Plaza. It is also possible that the current library site might instead accommodate a different type of new development, other than a hotel and cultural center/museum. While current demand for hotel in the area may be low, in the longer term this could become an important component of activating the Civic Center and Downtown Compton in general. In the shorter term, Compton-specific cultural exhibitions could be held in the library and at City Hall to begin to generate interest, while fundraising and partnerships begin to materialize around the museum component, and improvements in Downtown Compton’s environment increase the demand for hospitality uses.



409 N. Alameda St.

This currently vacant City-owned site is adjacent to the Metro at Compton Senior Apartments, a 75-unit senior affordable housing development completed in 2015. It was included in a City RFP in 2018 along with the 501/601 E. Compton Boulevard site; however, no Exclusive Negotiating Agreement has been signed regarding this site. As a result, the site will be subject to the updated Surplus Land Act and will need to be offered first for affordable housing development.

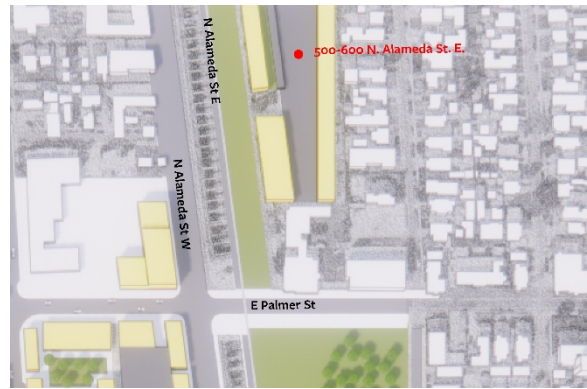
The Specific Plan anticipates a project of 106 affordable residential units on this 0.8-acre site in a five- or six-story structure which would ideally have a lower height on the northern portion of the property, building up to a taller height and small commercial component at the corner Alameda Street and Carson Place, across the street from where a key downtown gateway element is envisioned on the southwest corner of the street.



500-600 N. Alameda St. E.

This 3.6-acre site is also used as a yard by the City Public Works and Parks and Recreation Departments. There are a few structures on-site, including 1940's-era Quonset huts; however, all are in fair to poor condition. The northern portion of the narrow site is suited to townhomes, which could be developed as affordable homeownership units, while the wider southern portion could be used for a multistory building with ownership or rental flats.

The project could leverage outside funds for the creation of the Alameda East greenway (Section 5.5), which would greatly enhance the livability of the development itself. Due to the narrowness of the site, buildings will likely come up to the property line, and a portion of the greenway can be used to create a buffer for the residential units.



458 S. Alameda St. E.

This 5.5-acre site is currently used by the City Public Works Department as a yard for City vehicles. The Specific Plan proposes consolidating the vehicles on this site into a three-acre footprint, freeing roughly two acres to be divided between affordable housing development and park or community garden space. Soil sampling should be conducted to determine the best portions of the site for development. One potential for this green space, strongly supported by the community, is to combine the community garden with a “food incubator,” a building which would provide rentable commercial kitchen space to small food vendors, including those who currently operate out of their homes.

The Specific Plan envisions a four-story building with 90 residential units on the site. This would be an appropriate location for affordable family housing.



398 N. Willowbrook Ave. E.

This one-acre City-owned parking lot just northeast of the Metro station and north of the Dollarhide Center parking garage is a prime opportunity to create a transit-oriented affordable housing development. Given its small L-shaped footprint, constructing garage parking while maintaining a walkable first-floor frontage on this site will be difficult; therefore, this project is envisioned as a small-unit affordable housing development without parking for seniors, single workers, or other populations more likely to live without access to a vehicle. The project could leverage grant funds for the proposed conversion of Willowbrook Avenue into a couplet with bicycle facilities (Section 5.4).

Just south of the parking lot, on the west façade of the Dollarhide parking garage, are a grassy berm and a new Metro security building which is vacant. The grassy berm represents an opportunity for the City to encourage pop-up or convenience retail, perhaps in a simple stand or shipping container. The Metro security building could be occupied by the Business Improvement District as offices and a hub for maintenance and security services.



7.4 Arts and Culture Promotion

The Specific Plan envisions Downtown Compton as a hub for cultural production and as a world-class destination to experience art and culture. Key arts and culture related projects include:

- Streetscape customization of the Compton Walk with locally-directed artistic, historic, interactive, and didactic elements along the streetscapes and in Major and Minor ‘Places’ (Section 2.7, 4.14, 4.15)
- Creation of an innovation hub for digital production in the 501/601 E Compton Blvd project (Section 2.6)
- Development of a hotel, museum and/or cultural center at the Civic Center celebrating area(s) in which Compton has contributed to the world (Section 2.4)

The plan also puts regulations in place to direct 0.5% of development project costs into public art (Section 3.10) and establish public benefit incentives for affordable arts and culture space (Section 3.12).

Artistic and cultural industries are often decentralized and collaborative, and in order to achieve these goals, the City will need to engage with a broad range of organizations and individuals, some of whom may take a larger role than the City itself. One potential method to stimulate collaboration between residents, City leadership, and these organizations would be to establish

a City Arts Commission and appoint members of the key local arts organizations to it. The Arts Commission could also relieve the Planning Commission of duties assigned in this Specific Plan related to the review of applications for public art in Section 3.10.

Funding for arts and culture projects may come from competitive public grant sources like the ones mentioned in Section 7.6; however, philanthropy is also a promising avenue. Famous current or former Compton residents such as Dr. Dre and the Williams sisters have made substantial contributions to cultural and sports facilities in the community. More institutional foundations may also be interested. Finally, large non-profit institutions such as universities also present opportunities to develop and operate of cultural and educational facilities. The key is to identify lead departments or organizations, inside or outside the City and quasi-governmental bodies, that have the drive and capacity to seek, coordinate, and execute funding.

One potential is the Compton Arts Project, a collaborative between SEPIA Collective, Cakecutter Institute, the Robey Theatre Company and the Mayor's Office of the City of Compton which has received funding from public and private sources. Its five goals are to:

- 1) Center arts and culture in the City
- 2) Fund an arts and culture community center in the City
- 3) Highlight and support the brilliance of Compton artists and their contributions
- 4) Encourage local ownership and development of businesses near our cultural centers
- 5) Demonstrate the value of art as an economic engine that has the power to anchor communities

Specific Plan Projects such as the Compton Walk, potential affordable arts or community space in new development, and the establishment of a cultural center in the Civic Center align clearly with the goals of the Compton Arts Project.

7.5 Historic Resources

The City of Compton does not currently have a local historic designation program. The City might consider establishing a local historic designation program and placing key historic resources such as the MLK Memorial Plaza and Compton Courthouse in it, or applying for designation for these publicly-owned resources. This would make these areas eligible for federal and state Historic Tax Credits, Historic Preservation Certified Local Government Program (CLG) grants, and other sources which can be used for revitalization.

7.6 Funding Sources

Figure 7-1 gives a list of some of the most promising funding sources for implementation of the Specific Plan as of early 2022.

Funding/Implementation Method	Purpose	Funding Type
City		
Specific Plan Costs Recovery Fee Program	Recouping incurred non-grant costs for plan preparation incurred by the City.	One-time fees on developers realizing benefits from CEQA-approved specific plan development provisions.
Financing Districts:		
Community Facilities Districts (CFD) (Mello Roos)	Capital improvements and operations & maintenance.	2/3 Voter - Approved annual special tax on property owners for identified purposes; can issue bonds.
Special Benefits Assessments	Capital improvements and services.	Simple majority fair-share special assessment of costs to benefiting property owners with the option of paying over time, qualifies for bond issuance.
Landscaping and Lighting Maintenance Districts (LLMD)	Capital improvements and operations & maintenance.	Type of special assessment for landscaping and lighting
Enhanced Infrastructure Financing Districts (EIFD)	Area-wide infrastructure district	Tax-increment financing to fund bonds; Specific rules for establishment, financing horizon and approved projects.
Community Revitalization and Investment Authority (CRIA)	Area-wide affordable housing and infrastructure district (Applicable to SB 375 Disadvantaged Communities)	Tax-increment financing to fund bonds; Specific rules for establishment, financing horizon and approved projects.

Fees:		
Park Fees	Capital expenses for park and open space	One-time fees on developers based on the Quimby Act, in lieu of park/open space dedication by developer.
Development Impact Fees	Variety of Capital Improvements	One-time fees on developers developed on a fair-share nexus basis that finance a variety of capital facilities and improvements.
State Gasoline Tax - City Capital Improvement Program (CIP) Allocations	Street improvements and maintenance.	Annual allocation from state to local jurisdictions based on formula.
Business Improvement District (Business and Property)	Operations and maintenance of common facilities, and small-scale capital projects, including parking facilities.	Self-elected annual assessments on businesses or properties; cannot issue bonds, thus limited on the scale of capital expenditures.
County		
Measure A: Safe, Clean Neighborhood Parks & Beaches	Grant Funding for park and open space improvement and creation.	Annual allocations and competitive grants of the annual Measure A special tax of property.
Measure W: Safe Clean Water Program	Provides local, dedicated funding to increase local water supply, improve water quality, and protect public health.	Annual allocations and competitive grants of the annual Measure W special tax of property.
Metro Active Transportation (MAT)	Grants for first/last mile and active transportation corridor projects; focused in disadvantaged and high transit ridership areas.	Grant allocation through a competitive process.

Metro Affordable Transit Connected Housing (MATCH)	Loans for predevelopment funds and acquisition for new affordable housing projects and for protection of tenants in existing rental properties that may be redeveloped	Subsidized loan to developer
State		
SB 1: Road Maintenance and Rehabilitation Program	California State Transportation Fund for Local Street Maintenance and Repair	Allocation to Local Agencies based on an apportionment formula
Affordable Housing and Sustainable Communities (AHSC)	Funding for greenhouse gas reduction projects that combine affordable housing development with active and public transportation improvements	Grants made to developers through a competitive process. City support required.
Active Transportation Program	Funding to increase the proportion of trips accomplished by walking and biking, increasing the safety and mobility of non-motorized users, advancing efforts of regional agencies to achieve greenhouse gas reduction goals, enhancing public health, and providing a broad spectrum of projects to benefit many types of users including disadvantaged communities	Grant allocation through a competitive process.
Infill Infrastructure Grant (IIG) Program	California HCD grant financial assistance for Capital Improvement Projects that are an integral part of, or necessary to facilitate affordable housing development.	Grant made to developers through a competitive process. City support required.

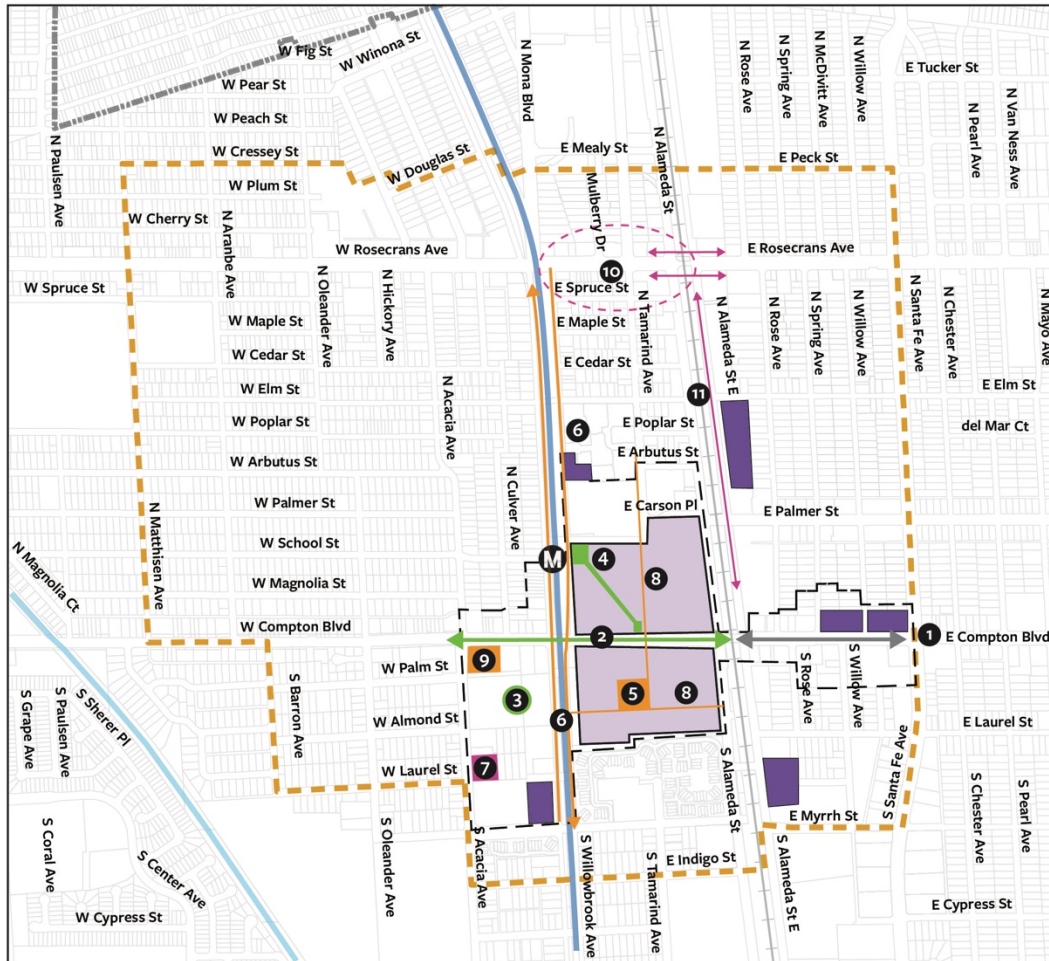
California Urban Greening Program	California Natural Resources Agency financial assistance for greenhouse gas and heat island reduction (Applicable to SB 375 Disadvantaged Communities)	Grant allocation through a competitive process
California Arts Council Grants	Various Council grants	Grant allocation through a competitive process.
Historic Preservation -- Certified Local Government Program (CLG) Grants	State of California (OHP), and the National Park Service (NPS) grants for historic preservation.	Grant allocation through a competitive process.
Highway Safety Improvement Program	Federal-aid program to States for the purpose of achieving a significant reduction in fatalities and serious injuries on all public roads.	Grant allocation through a competitive process.
Federal		
Community Development Block Grants	HUD Capital improvements and operations & maintenance.	Annual allocations to cities to revitalize neighborhoods, expand affordable housing and economic opportunities, and/or improve community facilities and services,
RAISE -- Rebuilding American Infrastructure with Sustainability and Equity, 2021	Transit-supportive area-wide improvements under the 2021 Bipartisan Infrastructure Law	Grant allocation through a competitive process.

Opportunity Zone (OZ) Funds	Private capital investment for a variety of housing and economic development projects (Applies to designated OZs within Compton: within the Specific Plan area these are SW of the corner of Compton Blvd & Willowbrook Ave and north of Rosecrans Ave)	Private investors realize capital gains tax deferral on investments made within Federal OZs made through OZ Funds.
New Markets Tax Credit (“NMTC”) areas	Private capital investment for a variety of housing and economic development projects (Applies to the NMTC Qualified Area within Compton).	Incentivizes private investment in distressed areas through tax credits. Investments made through Community Development Entities (CDE).
Non-Profit		
California ReLeaf Program	Non-Profit Partnership with CA Dept. of Forestry and Fire Protection	Grant allocation through a competitive process.

Figure 7-1. Funding Sources.

7.7 Capital Project Implementation

Figure 7-2 provides a map of the anticipated capital projects to implement the Specific Plan and presents the current priority level assigned to each project. Figure 7-3 then provides funding sources which may be pursued to fund each of these capital improvement projects.



Capital Projects

- ① Compton Walk Phase 1: road diet, streetscape
 - ② Compton Walk Phase 2: road diet, streetscape
 - ③ MLK Plaza Improvement (Addition of Rentable Kiosks, Seating, Greenery)
 - ④ Metro Plazas & Pedestrian Mall
 - ⑤ Compton Central Park
 - ⑥ Willowbrook Ave Conversion to Couplet & Bike Facilities
 - ⑦ Conversion of Open Space to Neighborhood Park
 - ⑧ New Streets
 - ⑨ Museum & Hotel
 - ⑩ Relocation of Sheriff Station (land purchase required)
 - ⑪ Alameda East Street Closure/Greenway and Connection of Rosecrans Av across Alameda Corridor
- Funded
 - High Priority
 - Medium Priority
 - Low Priority
 - Public Affordable Housing Development Sites
 - Major Private Development Sites

Figure 7-2. Capital Projects

Map Key	Specific Plan Capital Projects	Development Condition?	Potential Funding Sources	Level
1	Compton Walk Phase 1: road diet, streetscape	Yes, per 501/601 SP	i. Funded with Affordable Housing and Sustainable Communities (AHSC)	State
2	Compton Walk Phase 2: road diet, streetscape	Possible	i. Financing Districts: CFD, LLMD, Special Benefit Assessment, EIFD, CRIA ii. Affordable Housing and Sustainable Communities (AHSC) iii. Infill Infrastructure Grant (IIG) Program iv. Active Transportation Program (ATP) v. RAISE -- Integrated with Area-wide Transit-Supportive Improvements vi. Highway Safety Improvement Program (HSIP)	City and/or Joint Powers Authority State State State Federal State
3	MLK Plaza Improvement (Addition of Rentable Kiosks, Seating, Greenery)	No	i. Historic Preservation -- Certified Local Government Program (CLG) Grants ii. California Urban Greening Program iii. California ReLeaf Program	State/Federal State Non-Profit
4	Metro Plazas & Pedestrian Mall	Yes	i. Private Developer Funds ii. Park Fees iii. Financing Districts: CFD, LLMD, Special Benefit Assessment, EIFD, CRIA	City City and/or Joint Powers Authority
5	Compton Central Park	Yes	i. Private Developer Contributions ii. Financing Districts: CFD, LLMD, Special Benefit Assessment, EIFD, CRIA iii. Park Fees iv. Measure A: Safe, Clean Neighborhood Parks & Beaches v. California Urban Greening Program vi. Community Development Block Grants vii. California ReLeaf Program	City and/or Joint Powers Authority City County State Federal Non-Profit

6	Willowbrook Ave Conversion to Couplet & Bike Facilities	No	i. Financing Districts: CFD, LLMD, Special Benefit Assessment, EIFD, CRIA ii. Affordable Housing and Sustainable Communities (AHSC) iii. Infill Infrastructure Grant (IIG) Program iv. Active Transportation Program (ATP) v. RAISE -- Integrated with Area-wide Transit-related Improvements	City and/or Joint Powers Authority State State State Federal
7	Conversion of Open Space to Neighborhood Park	Possible	i. Park Fees ii. Financing Districts: CFD, LLMD, Special Benefit Assessment, EIFD, CRIA iii. Measure A: Safe, Clean Neighborhood Parks & Beaches iv. California Urban Greening Program	City City and/or Joint Powers Authority County State
8	New Streets	Yes	i. Private Developer Funds ii. Financing Districts: CFD, LLMD, Special Assessment, EIFD, CRIA iii. RAISE -- Integrated with Area-wide Transit-related Improvements	City and/or Joint Powers Authority Federal
9	Museum & Hotel	N/A	i. Public-Private Partnership ii. Opportunity Zone Funds iii. New Markets Tax Credit (“NMTC”) areas iv. California Arts Council Grants	Local Federal Federal/ Community Development Entities State
10	Relocation of Sheriff Station (land purchase required)	No	i. Development Impact Fees ii. Financing Districts: CFD, LLMD, Special Benefit Assessment, EIFD, CRIA iii. Other City Funds -- Sale or Lease of Existing Facilities	City City City

11	Alameda East Street Closure/Greenway and Connection of Rosecrans Av across Alameda Corridor	No	<ul style="list-style-type: none"> i. Financing Districts: CFD, LLMD, Special Benefit Assessment, EIFD, CRIA ii. Measure A: Safe, Clean Neighborhood Parks & Beaches iii. Measure W: Safe Clean Water Program iv. Affordable Housing and Sustainable Communities (AHSC) v. Infill Infrastructure Grant (IIG) Program vi. California Urban Greening Program vii. California ReLeaf Program viii. RAISE -- Integrated with Area-wide Transit-related Improvements 	<ul style="list-style-type: none"> City and/or Joint Powers Authority County County State State State Non-Profit Federal
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Figure 7-3. Capital Improvement Projects with Funding Sources.

Appendix: Compton Walk Streetscape Design Concept

This appendix describes the streetscape design concept for the Compton Walk which was developed by The Arroyo Group in consultation with representatives of the Compton Station Specific Plan Stakeholders Committee.

A.1 Streetscape Design Concept and Theme

As a nod to Compton's inspiring musical history, The Compton Walk will include a base streetscape patterning and design intended to resemble a blank musical 'score sheet', upon which the local community will be encouraged to compose and share their own unique 'songs' and expressions about Compton. The experiences along the Walk will be created, maintained, and updated over time by local artists and business owners, expressing the City of Compton's contributions to music, sports, politics, theater, visual arts and fields yet undefined. It will serve as a major attraction to those who wish to come to appreciate and explore the City's culture.

Figure A-1 illustrates diagrammatically how a range of programming and activities can be composed to generate a unique and evolving range of experiences, similar to how notes can be combined to make a song. Punctuating this primary pathway of experiences will be a rhythm of streetscape 'Places' associated with adjacent private development and other civic open spaces that will seek to promote additional moments of connection, discovery, learning, history, cultural expression, and entertainment.

Figure A-1 shows the overall extents and routing of the Compton Walk. Key districts and civic spaces will be connected and a regular placement and hierarchy of programming and space types will promote exploration and discovery.

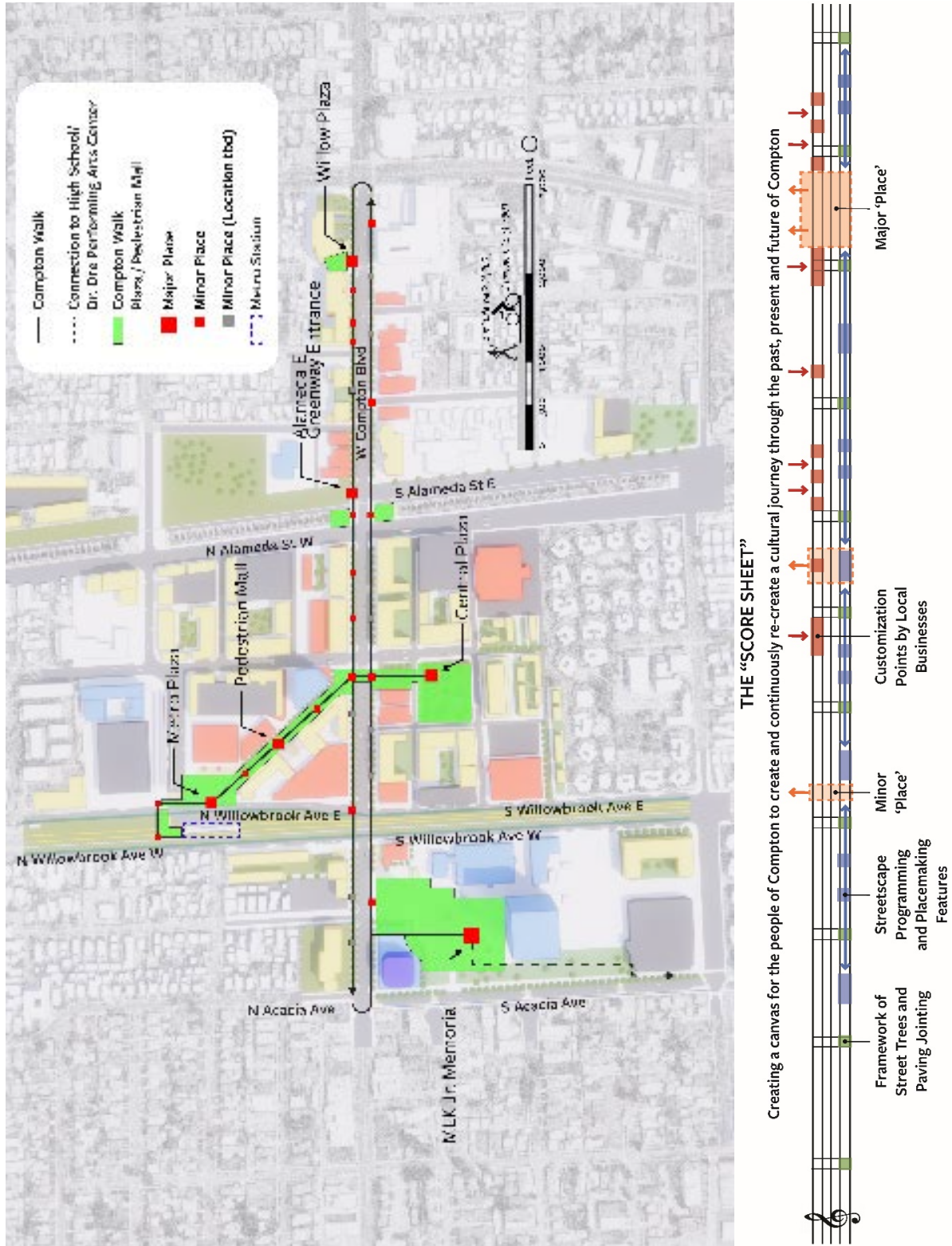


Figure A-1. Overall Compton Walk Diagram (left) and 'Score Sheet' Programming Concept (right).

A.2 The 'Score Sheet' Base Streetscape Improvements.

The following base streetscape improvements have been proposed along Compton Blvd to create the foundation to a new walkway experience that will evoke feelings one is walking down a musical 'score sheet' and discovering the unique and evolving song which the community will 'compose'.

The base 'score sheet' streetscape improvements include:

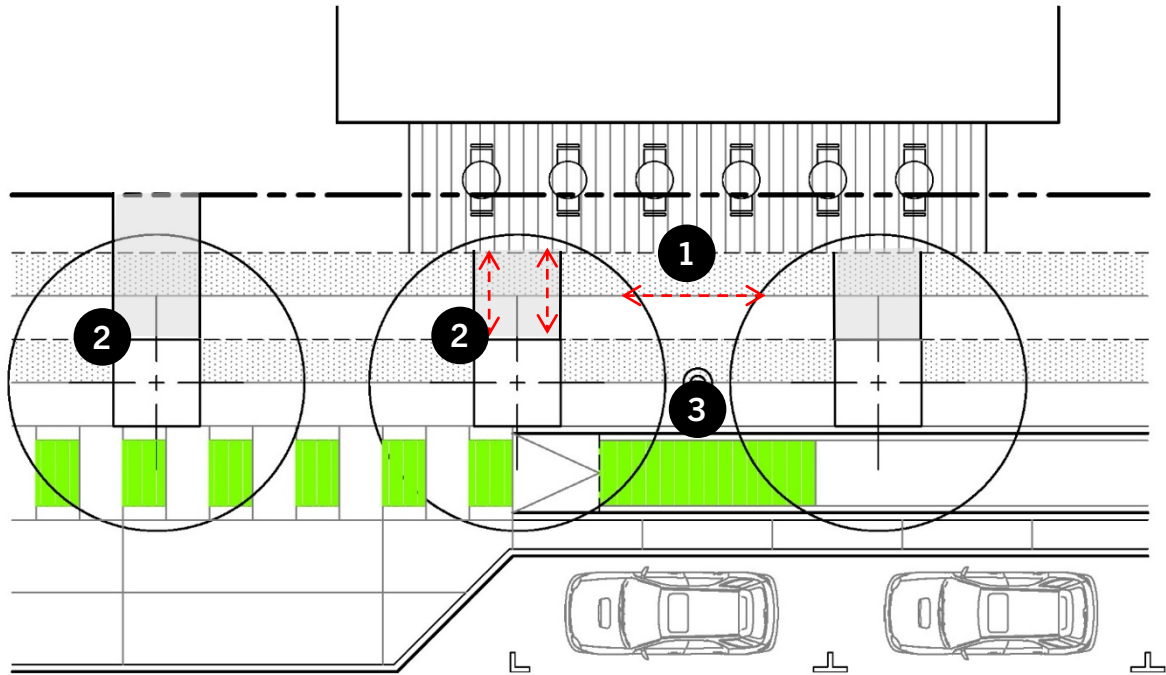
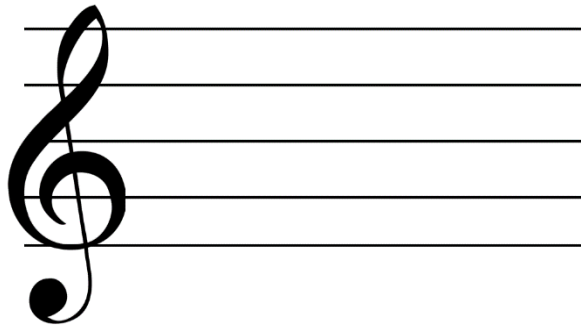


Figure A-2. Typical 'Score Sheet' Streetscape.

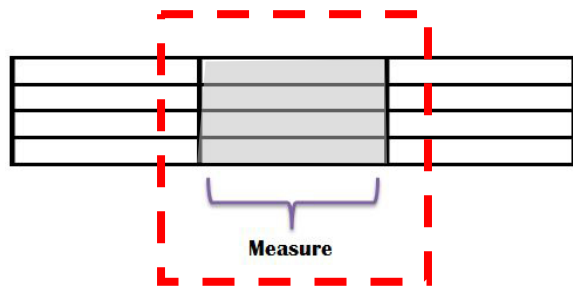
1. Linear Walkway Paving Patterning (the ledger lines of the 'score sheet')

Long, linear pavement scoring lines are intended to help maintain a cohesive-looking streetscape along the length of corridor while also providing a framework for the organization and future placement of art and other programming elements (aka 'notes', see below). Sawcut concrete joints will provide a clean, cost-effective, and easily implementable system for patterning the concrete sidewalk surface and will become a simple backdrop for art and placemaking interventions to stand out as featured elements. Ease of maintenance will be another key feature of the proposed sidewalk treatment by utilizing durable concrete materials, that will be easy to clean and repair, with a simple joint patterning that can be replicated across a range of sidewalk conditions and material lifecycles.



2. Streetscape Sub-Division Framework (the measures, or bars, of the 'score sheet')

A regularly spaced, repetitive system for segmenting the long direction of the Compton Walk will help further unify the corridor while also providing a sequence of smaller places for scaling up or down a hierarchy of new streetscape moments, experiences, and placemaking (see more under Sections 4.14 and 4.15). At-grade street tree planters and perpendicular-to-curb sidewalk jointing will be the primary sub-dividing features for this layer of the streetscape design.



3. Streetscape Furniture (the beat)

New streetscape furniture elements will help provide a starting point to activate and engage people along the corridor. A range of seating types and formations will be important to provide for a variety of user needs, from contemplative individual seating to more social group seating. Other street furniture elements also include trash receptables, pedestrian lighting, bike racks, signage and wayfinding, district branding, and other items TBD. A specific aesthetic direction and/or Supplier for the range of identified furniture should be selected during the next phases for design and in conjunction with a more refined community outreach process.



A.3 Streetscape Customization: Historic, Art, and Placemaking Features

Building on the base 'Score Sheet' streetscape, the Compton community will be encouraged to compose its own melody of artistic, historical, and interactive elements which will be the focus of a journey through cultural Compton. The process of developing the placemaking will need to be highly inclusive and consider a range of perspectives, weaving different elements and layers together to create a cohesive song of experiences. A strong community engagement process is encouraged to ensure all voices are heard and registered in composing a vision for The Walk.

Streetscape Programming and Placemaking Features

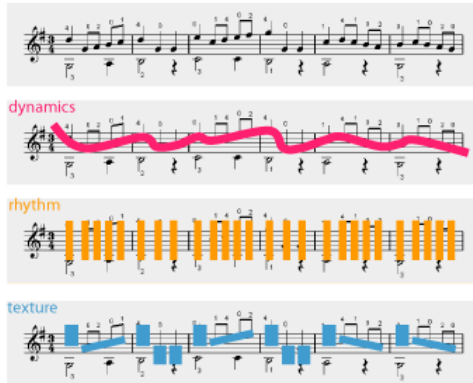
A composition of streetscape programming, art, placemaking, and other ideas will be important layers for Compton Walk to be able to tell its own story and unify an overall walk experience. These layers may include a regular rhythm of elements to hold the streetscape together and other unique, localized placemaking moments to promote further exploration and discovery.

Goals for programming and the development of placemaking along The Walk:

- Develop a range of elements and placemaking that cater to the needs of abutting businesses, the general resident community, the historic community, the creative community, and outside visitors with the goal to serve as broad of a range of user types as possible and in turn increase vibrancy.
- Provide a variety of activities for different types of users, activities and times of day, also to increase vibrancy.
- Balance elements made to pay tribute to the past with creative, forward-thinking elements and messaging for the future.
- Educate people about the lesser-known stories of Compton's past, while also responding to the general public's love for famous members of the community.
- Provide a range of element types, including vertical elements, multi-media, and lighting to provide visual strong interest.



A range of program types encourage to create vibrancy. A regularly spaced rhythm of program elements to unify the streetscapes (left), one-off placemaking interventions and art (right)



This visualization shows how different combinations of programming (notes) can create very different experiences (songs) along the Compton Walk streetscape and is encouraged.

Minor ‘Places’

This guideline will allow for and encourage local businesses to utilize portions of The Compton Walk streetscape to better engage with the overall walkway experience and promote a clearer connection and gateway to those businesses. Outdoor dining operations, unique business paving and patterning, and other business improvements will be allowed to push out into the edges the streetscape, identified as Streetscape Zone C.

A series of these smaller enhanced streetscape spaces are envisioned to dot the Compton Walk and act as ‘breadcrumbs’ for encouraging further exploration and discovery throughout the downtown. Minor ‘Places’ will be distinguished from the rest of the streetscape with artistic or special paving elements related to the adjacent business operations. Minor ‘Places’ would be located at key streetscape nodes or building entries to mark adjacent destinations and access points. Several suggested locations have been identified in **Figure 2-10** in red with provisions in gray for others to be implemented over time.



Source: The Apollo Apartments,



Major 'Places'

The Compton Walk will provide important connections to the downtown's key civic spaces and parks, which will be experienced as Major 'Places' as part of a comprehensive walk experience. Large, iconic programmatic elements, events, art celebrating Compton greats, and vibrant civic spaces will be featured at these locations to help focus the energy of the downtown at its most important centers. Some of the downtown locations identified for Major 'Places' include, but are not limited to:

- MLK Memorial Plaza (Civic Center) – see Section 2.4
- Metro Plaza (adjacent to Compton Station) – see Section 2.5
- Corner of Compton Blvd. and Tamarind Ave. – see Section 2.5
- Alameda East Greenway Gateway – see Section 2.9
- Willow Plaza – see Section 2.6



A.4 Greening Features

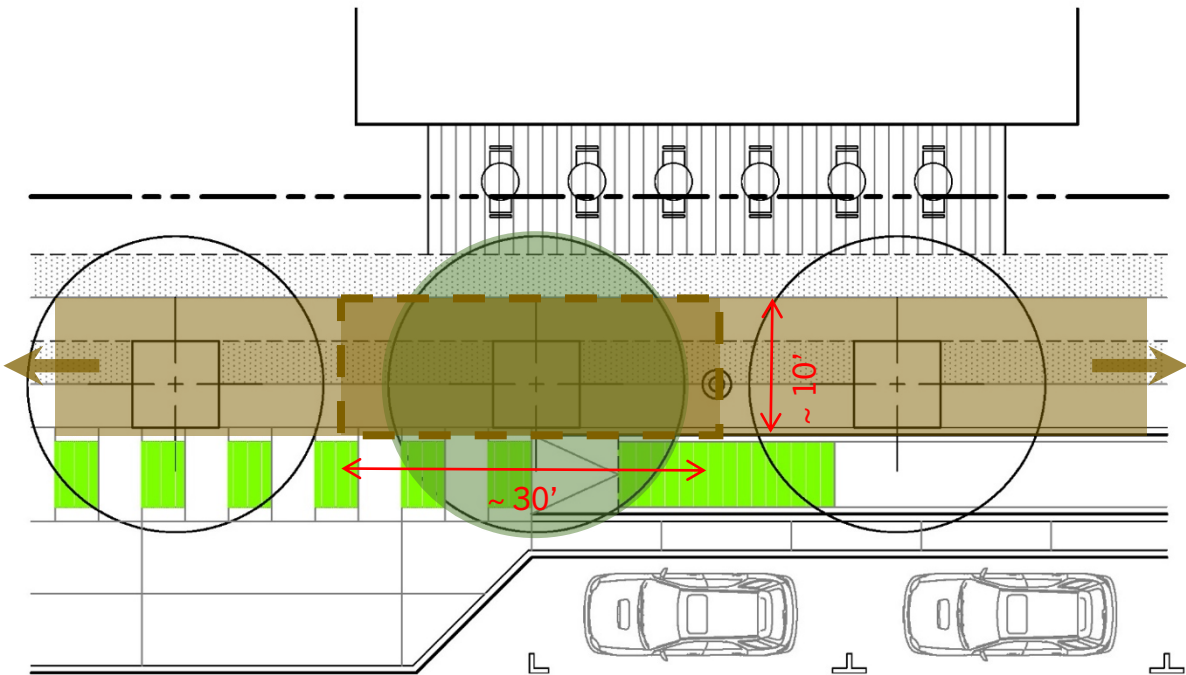


Figure A-3. Compton Walk Landscape Infrastructure Systems.

Street Trees - Rooted in Compton's Local History.

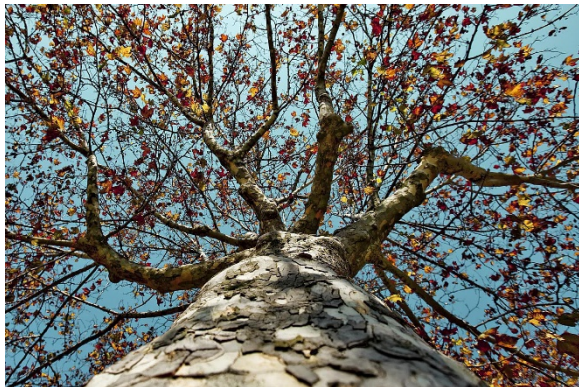
The tree selection for Compton Boulevard is based on the historic Eagle Tree in Compton, which predated the founding of the City of Compton and was a nod to the City's historic rancho origins.

The Eagle Tree, which was located at the southeast corner of Poppy Street and Short Avenue, dated back to the mid-1800's where the tree, already mature at that time, was used as a border marker at the northeastern boundary of the historic Rancho San Pedro, a 118-square-mile tract that covered most of what is now the South Bay and Compton areas, as well as the western part of Long Beach. The tree was used by travelers as an orientation marker and was given its name by eagles that used it as a perch point overlooking the broad surrounding landscape. This large California Sycamore was one of the few remnants of a long past era of California history, however, it was cut down six years ago when it became sick and a hazard. Today the Eagle Tree stump remains with a plaque.

London Planetree (*Platanus acerifolia* 'Columbia') was chosen as the featured street tree for Compton Blvd due to its close similarities to the native California Sycamore (*Platanus racemosa*) and because of its better overall suitability as a streetscape tree, with a more upright growth habit and superior disease resistance. London Planetree was also favored for its large leaves and thick canopy, providing shade during the hottest months of the year. The trees' deciduous nature will provide interesting color changing foliage in fall and, with no leaves, allow warming sun to shine through the canopy in the cooler winter months.



The Eagle Tree – Then.



London Planetree (Sycamore).

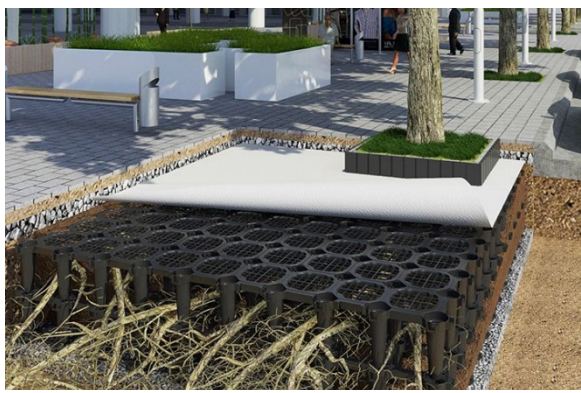


Root Trenches - Infrastructure for Supporting Large Street Tree Growth.

Growing large-size street trees in constrained, urban sidewalk conditions is contingent on the trees having enough below-ground soil volumes to support the large growth. A root trench is a landscape infrastructure system that allows for the maximization of planting soil volume in tight urban conditions.

Root trenches provide a long, narrow volume of soil along the length of the streetscape (parallel to the curb line) and provide an unobstructed below-ground medium for urban street tree roots to grow in. The growth medium for tree roots does not need to be evenly distributed around the tree trunk to be effective, it may be stretched out in a linear direction and still provide a similar, usable soil volume. The root trench system takes advantage of spaces between trees (often under sidewalk hardscape) as part of maximizing the available root growth zone and provides a means of co-sharing root zone spaces between trees. A target of 1,000 cubic feet of planting soil will provide sufficient growth medium for a single 30-foot diameter street tree and will require a roughly 10' wide subterranean root trench.

Maximizing aeration points to the root zones below paving areas will help facilitate root growth and may require a porous paving surface or regular openings in the surface sidewalk. Below-ground structural soil and cellular support systems are often used to support surface paving over root trench zones and would be further developed in detailed design stages for the streetscape implementation. Root trenches also provide excellent opportunities as infiltration points for rainwater and corridor drainage.



Source: WT Burden Landscape.